



City of

SPRING HILL

TENNESSEE

est. 1809

AGENDA PACKET

Municipal Planning Commission

Planning Commission

January 26, 2026

05:30 pm



SPRING HILL MUNICIPAL PLANNING COMMISSION

Elizabeth Droke, Chairman
Sam Isbell
Matt Fitterer, Mayor
Amanda Rondeau

Ryan Connolly, Vice Chairman
Greg Groninger
Jaimee Davis, Alderman

City of Spring Hill
199 Town Center Parkway
Spring Hill, TN 37174

AGENDA PLANNING COMMISSION

January 26, 2026
05:30 pm

A. CALL TO ORDER

B. ROLL CALL

C. CHAIRMAN COMMENTS

Audience members wishing to speak to an agenda item will have the opportunity to speak at the beginning of the agenda and will have five minutes to address the Planning Commission. No rebuttal remarks are permitted. For those of you who are not able to attend the in-person meetings, please submit your public comments regarding agenda and non-agenda items to PCPublicComment@springhilltn.org.

D. PUBLIC COMMENT (NON-AGENDA ITEMS)

E. DRAFT DECEMBER 8, 2025, PLANNING COMMISSION REGULAR MEETING MINUTES

Attachment: [DRAFT Regular Meeting Minutes 12.8.25.pdf](#)

F. BONDS

1. PC Resolution 26-1 to Release the Existing Performance Bond and Establish a Maintenance Bond for Sanctuary Bluff Apartments
2. PC Resolution 26-2 to Release the Maintenance Bond for Sanctuary Bluff Apartments
3. PC Resolution 26-3 to Release the Maintenance Bond for Barton Hills Phase 1A
4. PC Resolution 26-4 to Dedication of Road ROW and Public Utilities Barton Hills Phase 1A
5. PC Resolution 26-5 to Release the Existing Performance Bond and Establish a Maintenance Bond for Brandon Woods Traffic Signal
6. PC Resolution 26-6 to Establish a Performance Bond for Legacy Pointe Phase 1A

Attachment: [January Bond Packet.pdf](#)

G. OLD BUSINESS

H. NEW BUSINESS

1. STP 1965-2025

Submitted by Kimley Horn for Platinum Storage. The property is located on the north side of Beechcroft Road, east of Dr. Robertson Road, best known as 830 Beechcroft Road. It is zoned I1 and is approximately 7.67 acres. The applicant is requesting site plan approval for the expansion of the existing storage facility, which includes covered RV and boat storage. Planner: Kali-Flynn Burke

Attachment: [STP 1965-2025 - Platinum Storage - sr.pdf](#)

Attachment: [BeechcroftLLC PlatinumStorage SitePlans 2026.01.20.pdf](#)

Attachment: [Platinum Storage Center Spring Hill Sewer Demand Letter.pdf](#)

2. ADM 1973-2025

Submitted by Andrew Kozlowitz for 2981 Stewart Campbell Pointe. The property is located on the south side of Stewart Campbell Pointe, to the west of Hunt Valley Drive. It is zoned R-2 and is approximately 0.249 acres. The applicant is requesting an easement reduction from 20' to 10' for the rear PUDE. Planner: Gil Morin

Attachment: [ADM 1973-2025_2981 Stewart Campbell Pointe_sr.pdf](#)

Attachment: [PUDE Application.pdf](#)

Attachment: [Cherry Grove Addition Ph-5.pdf](#)

I. OTHER BUSINESS

1. * Discussion of June Lake, POD K, Bonds

2. Election of Officers

J. ROUND TABLE

K. ADJOURN

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**SPRING HILL
MUNICIPAL PLANNING COMMISSION
REGULAR MEETING MINUTES
MONDAY, DECEMBER 8, 2025
5:30 P.M.**

A. CALL TO ORDER

Chairman Droke called the meeting to order at 5:30PM.

B. ROLL CALL

Members Present: Mayor Matt Fitterer, Chairman Liz Droke, Vice Chairman Ryan Connolly, Commissioner Jamiee Davis, Commissioner Sam Isbell, Commissioner Greg Groninger, and Commissioner Amanda Rondeau was absent.

Staff Present: Senior Planner Gilles Morin, City Engineer Karol Czerwonka, Associate Engineer Ryan Turley, Associate Planner Kyle Spires, Associate Planner Kali-Flynn Burke, Assistant Planner Tobias Faucher, Engineer Gerald Bolden, Planning Tech Lucretia Sappington, and City Attorney Patrick Carter.

C. CHAIRMAN COMMENTS: *“Audience members wishing to speak to an agenda item will have the opportunity to speak at the beginning of the agenda and will have five minutes to address the Planning Commission. No rebuttal remarks are permitted. If you have any comments regarding agenda and non-agenda items, please submit your public comments to PCPublicComment@springhilltn.org.”*

D. PUBLIC COMMENT (NON-AGENDA ITEMS)

E. PUBLIC COMMENT (AGENDA ITEMS)

F. ACCEPTANCE OF MEETING MINUTES NOVEMBER 10, 2025, REGULAR MEETING MINUTES.

Mayor Fitterer made a motion for approval of November 10, 2025, meeting minutes. The motion was seconded by Commissioner Connolly. The motion to approve November 10, 2025, meeting minutes passed by a vote of 6-0.

G. APPROVAL OF THE AGENDA

Commissioner Droke moved STP 1901-2025 SH Pediatric Dentistry from the Consent Agenda to New Business. Mayor Fitterer made a motion to approve the agenda as modified. The motion was seconded by Commissioner Connolly. The motion to approve the modified agenda passed by a vote of 6-0.

H. CONSENT AGENDA

1. PC Resolution 25-97 to Establish a Performance Bond for June Lake Pod E Section
 2. PC Resolution 25-98 to Establish a Performance Bond for June Lake Pod E Section 2 Landscaping
 3. PC Resolution 25-99 to Establish a Maintenance Bond for June Lake Pod E Section 2
 4. PC Resolution 25-100 to Establish a Performance Bond for Harvest Point Phase 18B
 5. PC Resolution 25-101 to Establish a Maintenance Bond for Harvest Point Phase 18B
 6. PC Resolution 25-102 to Release a Maintenance Bond for Dartford Townhomes
 7. PC Resolution 25-103 to Release a Performance Bond for Dartford Townhomes
 8. PC Resolution 25-104 Dedication of Road ROW and Public Utilities Dartford Townhomes
 9. PC Resolution 25-105 to Release a Maintenance Bond for Dartford Phase 2
 10. PC Resolution 25-106 to Release a Performance Bond for Dartford Phase 2
 11. PC Resolution 25-107 Dedication of Road ROW and Public Utilities Dartford Phase 2
 12. PC Resolution 25-108 to Release a Performance Bond for June Lake Phase 1 Pod D
- 13. PDF 1941-2025:** Submitted by the Richmond Company for Spring Hill Commerce Center. The property is located to the east of the I-65 and to the south of Jim Warren Road. It is zoned PD with a base zoning of I-2 and is approximately 86 acres. The applicant is requesting planned development final approval for approximately 204,000 sf building that is a portion of Phase 1 of the approved Spring Hill Commerce Preliminary Development Plan. **Planner: Kyle Spiers**
- 14. STP 1930-2025:** Submitted by Timmons Group for Wells Fargo. The property is located at 4933 Main Street on the west side of Main Street, and the south side of Campbel Station Parkway. It is zoned C-4 and contains approximately 1.20 acres. The applicant is requesting site plan approval for a financial institution with a drive-through ATM. **Planner: Kyle Spiers**
- 15. DRC 1942-2025:** Submitted by Bryan Price Engineering for Music City Auto Auction. The property is located on the north side of Royal Park Boulevard, to the east side of Kedron Road. It is zoned I-1 and is approximately 40 acres. The applicant is requesting design review for an approximately 1,040 sf metal building. Design Review only. **Planner: Tobias Faucher**
- 16. STP 1972-2025:** Submitted by the City of Spring Hill via Carollo for the Spring Hill Centrifuge Dewatering Project. The property is located to the northeast of Kedron Road, known as 3893 Mahlon Moore Road. The property is approximately 43.17 acres and is currently zoned IC. The applicant is requesting site plan approval for construction of new dewatering facility on the existing wastewater treatment plant site. **Planner: Kali-Flynn Burke**
- 17. FPL 1970-2025:** Submitted by SEC, Inc. for Legacy Pointe, Phase 1. The property is located east of The Crossings shopping center, west of Kedron Road, and north of Saturn Parkway. It is zoned PD with a base zoning of C-5 and is approximately 158 acres. The applicant is requesting to make lot line adjustments and create three new lots. **Planner: Gil Morin**
- 18. FPL 1971-2025:** Submitted by Wilson and Associates for June Lake, Pod E, Section 2. The property is located to the east side of Buckner Lane at the proposed extensions of Huron and Deep Creek Drives. The property is zoned PZD, G-RMU and is approximately 7.4 acres. The applicant is requesting a final plat approval to create 33 townhome lots, 24 duplex lots, 1 open space and dedicate r/w and establish easements. **Planner: Kali-Flynn Burke**

Mayor Fitterer made a motion to approve consent agenda with all staff associated conditions of approval and findings of fact. Commissioner Connolly seconded the motion. The motion to approve the consent agenda passed by a vote of 6-0.

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I. OLD BUSINESS

1. **STP 1841-2025:** Submitted by Kimley-Horn and Associates for **Whataburger**. The property is located at 3004 Belshire Village Drive, on the west side of Main Street, and the south side of Belshire Way. It is zoned C-4 and is approximately 1.12 acres. The applicant requests site plan approval for a conversion and reuse of an existing vacant drive-through restaurant building. **Planner: Gil Morin**

Mayor Fitterer made a motion to approve STP 1841-2025 with all staff associated conditions of approval. Commissioner Connolly seconded the motion. Motion to approve STP 1841-2025 failed by a vote of 5-1. The yeavote was cast by Commissioner Isbell.

Mayor Fitterer made a motion to deny STP 1841-2025 for failure to comply with Articles 11 Article 13 and Article 16 of the UDC. Chairman Droke seconded the motion. Motion to deny STP 1841-2025 passed by a vote of 6-0.

J. NEW BUSINESS

1. **PDF 1969-2025:** Submitted by Manhard Consulting for **Legacy Pointe, Heavy Retail Building**. The property is located to the east of The Crossings Shopping Center, and to the north of Saturn Parkway, on Crossing Circle. It is zoned PD with a base zoning of C-5 and is approximately 44.8 acres. The applicant is requesting site plan approval for an approximately 166,740 sf building, and a fueling station with 12 dispensers, a canopy, associated parking, and landscaping. **Planner: Gil Morin**

Mayor Fitterer made a motion to approve PDF 1969-2025 with all staff associated conditions of approval, adding a ninth condition of approval that the applicant shall screen the bale and pallet area along with the battery storage area with the same or similar fence as the remainder of the site.

Standard Conditions:

1. *Planning Commission approval shall remain valid for a period of three (3) years, during which time all action must be taken in accordance with the Unified Development Code, Articles 13.1.E and 17.8.*
2. *Project shall be constructed as approved unless otherwise approved for modification. The modification may be denied if the proposal alters the proposed use, increases the overall area of the project, increases the density of the development, or increases any local government expenditure necessary to implement or sustain the proposed use.*
3. *Project shall be constructed in accordance with the City of Spring Hill’s adopted Major Throughfare Plan.*
4. *One (1) electronic and one (1) 11x17 hard copy of approved plan shall be submitted to the Planning Office no later than 45 days post the PC approval meeting date.*
5. *Each project access should be located and designated so that departure sight triangles and required stopping sight distance requirements are met, as specified by AASHTO. All intersections should be designed such that departure sight triangles are clear of all sight obstructions, including buildings, signs, vertical and horizontal curves, utility poles, trees, etc.*
6. *All project access and driveways shall meet the City’s UDC requirements for alignment, layout, design, and spacing as it relates to existing intersections, driveways, access locations, etc.*
7. *A designated truck route should be prepared for the site with appropriate signing and guidance for use of specific routes and access locations. For the designated routing, all turning radii should be designed to accommodate the typical truck size (WB-67 or WB-50) used by the end user of the facility.*

8. All infrastructure (roadways, utilities, etc.) shall be designed according to the City of Spring Hill Standards and Specifications and meet all requirements within the UDC.
9. The project must not locate the required landscaping within any utility or drainage easements.
10. The photometrics for the proposed development shall meet all the UDC standards and requirements.
11. The applicant must provide a plan and profile for all access drives, these access drives shall meet AASHTO, TDOT, and City of Spring Hill Standards.
12. All sidewalks and pedestrian ways shall meet current ADA requirements.
13. Erosion control measures meeting TDEC and all City regulations will be required to be provided prior to approval of construction plans by the Engineering and Public Works Departments.
14. Intersection sight distance and stopping sight distance calculations shall be shown for all access drives and roadways with a connecting project access. Intersection departure sight triangles per AASHTO shall be shown for all project accesses (vertical and horizontal).
15. A Fire Exhibit to show that the site meets the intent of the Fire Code Checklist, Engineering Checklist, and IFC must be provided.
16. Any proposed grading outside of the property line shall have a signed letter of consent or proof of temporary easement from the adjacent property owner prior to the issuance of a grading permit.
17. All storm sewer plans and profiles must comply with all City regulations and checklist requirements before the Engineering and Public Works Department will approve the construction plans.
18. All sewer plan and profiles meeting TDEC and City regulations and checklists will be required prior to approval of construction plans by the Engineering and Utility Departments. All lines greater than 6" will be required to be profiles.
19. All infrastructure (roadways, utilities, etc.) shall be designed according to the City of Spring Hill Standards and Specifications and meet all requirements within the UDC.
20. Any roadway improvements shall be designed according to the City of Spring Hill, TDOT, AASHTO, and MUTCD standards and specifications.
21. Typical sections for all paving shall be provided along with a Geo-Technical Report.
22. All stormwater calculations must meet the requirements of the UDC, as well as applicable state and federal regulations, and shall be submitted prior to approval of construction plans by the Engineering and Public Works Departments. Any site layout or building square footage changes necessary to meet these regulations will be reviewed by the City Planner or Development Director to determine whether the project must return to the Planning Commission for approval.
23. Any proposed retaining walls shall be submitted for review and approval by Building Codes simultaneously with engineering site plan reviews and must be approved before the issuance of a grading permit.
24. Record drawings of the development will be required prior to the issuance of the Certificate of Occupancy (COs) for all installed infrastructure. Final Record Drawing Submittal is required prior to the release of bonds and to include the full design sent in both electronic files and hard copies. The electronic files must be geographically reference to NAD83 and NAVD88 and provided in a format that can be integrated into the City GIS and must meet all City Requirements.
25. No sewer and water hookups shall be allowed until corresponding infrastructure is installed, tested, and accepted by the City.
26. Bonds will be required to be posted per the UDC, including bonds for final asphalt pavement, sidewalks, traffic control devices, streetlights, ditches and drainage systems.
27. Long-term maintenance/management plan that meets all UDC requirements, as listed in but not limited to Article 15.7, will be required prior to approval of construction plans by the Engineering Department

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and before a pre-construction meeting can be held.

- 28. The applicant must provide complete and sufficient information in the applicable design, satisfactorily addressing the City Engineer's comments before any permits or a pre-construction meeting can occur. Any disagreement on information, design, or accuracy of information between the City Engineer and applicant that cannot be resolved must be heard by the Planning Commission for final determination.*
- 29. Any additional requirements as determined during construction and engineering review for the sole purpose of meeting public health, safety, and welfare as determined by the City Engineer, Development Director, Public Works Director, and City Administrator will require field changes or site design revisions as necessary to fully address the concerns. Site layout or building square footage changes required to meet public health, safety, and welfare will be reviewed by the Planning Director or Development Director to determine if the project will be required to go back through Planning Commission approval process. The development representative will hold the right to go before Planning Commission if resolutions to address concerns between staff and the development cannot be agreed upon.*

Site Specific Conditions:

- 1. Required upgrades per the Traffic Access Study for Crossings Circle and Project Access #1:*
 - a. The access should be a two-way stop-controlled with the project access under stop control. All signing and pavement markings shall adhere to the latest edition of the MUTCD.*
 - b. The project's access should be constructed with one lane entering traffic and two lanes (one left-turn and one right-turn) for exiting traffic with a minimum of 75 feet of storage, measured from the stop line. This is to accommodate one WB-67 combination truck at the stop line without creating interference with operations at the first internal intersection, parking aisle or parking space.*
 - c. Per the City's UDC requirements (Article 16 Section C Table 16-2), a westbound right-turn deceleration lane with a minimum storage of 75 feet should be provided. The turn lane design, including tapers, shall be completed according to the AASHTO, TDOT, and the City of Spring Hill design standards and specifications. **NOTE:** the UDC required left-turn lane is accounted for by the center two-way left-turn lane on Crossings Circle.*
 - d. Per the City's UDC (Article 16 Section D.2.a), the current driveway spacing, for the Project Access #1 to the existing AMC Theaters driveway on the south side of Crossings Circle, does not meet the requirement of 200 feet of spacing for an arterial street. The driveway should be relocated to align directly with the AMC Theater driveway or shifted to the east to meet the 200 feet spacing. If shifted to the east, consideration should be given to the potential access for the development parcel on the south side of Crossings Circle.*
- 2. Required upgrades per the Traffic Access Study for Crossings Circle and Project Access #2:*
 - a. The access should be a two-way stop-controlled with the project access under stop control. All signing and pavement markings shall adhere to the latest edition of the MUTCD.*
 - b. The project's access should be constructed with one lane for entering traffic and two lanes (one left-turn and one right-turn) for exiting traffic with a minimum of 75 feet of storage, measured from the stop line. This is to accommodate one WB-67 combination truck at the stop line without creating interference with operations at the first internal intersection, parking aisle or parking space. The throat length may be reduced to 50 feet between the stop line and the first internal intersection, parking aisle or parking space, if the access is not part of the designated truck route for the site.*
 - c. Per the City's UDC requirements (Article 16 Section C Table 16-2), a westbound right-turn*

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*deceleration lane with a minimum storage of 75 feet should be provided. The turn lane design, including tapers, shall be completed according to the AASHTO, TDOT, and the City of Spring Hill design standards and specifications. **NOTE:** The UDC required left-turn lane is accounted for by the center two-way left-turn lane on Crossings Circle.*

3. *Required upgrades per the Traffic Access Study for Legacy Point Trail and Project Access #3:

 - a. *The access should be a two-way stop-controlled with the project access under stop control. All signing and pavement markings shall adhere to the latest edition of the MUTCD.*
 - b. *The project’s access should be constructed with one lane for entering traffic and one lane for existing traffic with a minimum of 75 feet of storage, measured from the stop line. This is to accommodate one WB-67 combination truck at the stop line without creating interference with operations at the first internal intersection, parking aisle or parking space. The throat length may be reduced to 50 feet between the stop line and the first internal intersection, parking aisle or parking space, if the access is not part of the designated truck route for the site.**
4. *Required upgrades per the Traffic Access Study for Legacy Point Trail and Project Access #4:

 - a. *The access should be a two-way stop-controlled with the project access under stop control. All signing and pavement markings shall adhere to the latest edition of the MUTCD.*
 - b. *The project’s access should be constructed with one lane for entering traffic and one lane for existing traffic with a minimum of 75 feet of storage, measured from the stop line. This is to accommodate one WB-67 combination truck at the stop line without creating interference with operations at the first internal intersection, parking aisle or parking space. The throat length may be reduced to 50 feet between the stop line and the first internal intersection, parking aisle or parking space, if the access is not part of the designated truck route for the site.**
5. *A traffic control plan, for work on any public roadway (Crossings Circle, Legacy Point Trail) open to travel at the time of construction of the improvements, will be required to be submitted to Engineering for review and approval.*
6. *Setbacks shall be shown from the future ROW on all applicable streets within the MTP.*
7. *All signage must meet UDC requirements, unless a variance is approved. No sign may obstruct any sight triangle.*
8. *Sewer shall not exceed 16,674 GPD and shall be tracked with flow meters approved by Spring Hill Water. It is the developers’ responsibility to purchase and install flow meters.*
9. *The applicant shall screen the bale and pallet area along with the battery storage area with the same or similar fence as the remainder of the site.*

Commissioner Connolly seconded the motion. Motion to approve PDF 1969-2025 passed by a vote of 6-0.

2. **STP 1901-2025:** Submitted by Catalyst Design Group for **SH Pediatric Dentistry**. The property is located on the north side of Beechcroft Road, known as 434 Beechcroft Road. It is zoned C-3 and is approximately 5.34 acres. The applicant is requesting site plan approval for a change of use from a single-family home to a dental office with a new parking lot. **Planner: Tobias Faucher**

Mayor Fitterer made a motion to approve STP 1901-2025 with all staff associated conditions of approval modifying the following site-specific conditions of approval:

- (1) *The applicant shall have six (6) months from site plan approval to execute the proposed lot split. If TDEC Consent Order WPC2025-0093 does not allow for the lot split to occur, then this condition shall be tolled until the closure of Consent Order WPC2025-0093.*

- (2) *The multi-use trail along the property shall be completed and constructed per UDC and ADA requirements. The eastern terminus of the multi-use trail shall be the proposed lot split line. In the event the applicant does not move forward with the proposed lot split, then the applicant shall construct the multi-use trail to the eastern property line.*

Standard Conditions:

1. *All street and infrastructure improvements must be complete prior to the City's issuance of Certificate of Occupancy (CO).*
2. *Right-of-way dedication shall be shown per the City of Spring Hill's Major Thoroughfare Plan for roadway classification and per requirements outlined in the UDC.*
3. *Any roadway improvements shall be designed according to the City of Spring Hill, TDOT, AASHTO, and MUTCD standards and specifications.*
4. *The photometrics for the proposed development shall meet all the UDC standards and requirements.*
5. *All sidewalks and pedestrian ways shall meet current ADA requirements. Spot elevations shall be provided to ensure there is ADA access to the building.*
6. *All infrastructure (roadways, utilities, etc.) shall be designed according to the City of Spring Hill Standards and Specifications and meet all requirements within the UDC.*
7. *The project must not locate the required landscaping within any utility or drainage easements.*
8. *The applicant must provide a plan and profile for all access drives, these access drives shall meet AASHTO, TDOT, and City of Spring Hill Standards.*
9. *Erosion control measures meeting TDEC and all City regulations will be required to be provided prior to approval of construction plans by the Engineering and Public Works Departments.*
10. *Typical Sections for all paving shall be provided along with a Geo-Technical Report.*
11. *Intersection sight distance and stopping sight distance calculations shall be shown for all access drives and roadways with a connecting project access. Intersection departure sight triangles per AASHTO shall be shown for all project accesses (vertical and horizontal).*
12. *A Fire Exhibit to show that the site meets the intent of the Fire Code Checklist, Engineering Checklist, and IFC must be provided.*
13. *Any proposed grading outside of the property line shall have a signed letter of consent or proof of temporary easement from the adjacent property owner prior to the issuance of a grading permit.*
14. *All storm sewer plans and profiles must comply with all City regulations and checklist requirements before the Engineering and Public Works Department will approve the construction plans.*
15. *All sewer plan and profiles meeting TDEC and City regulations and checklists will be required prior to approval of construction plans by the Engineering and Utility Departments. All lines greater than 6" will be required to be profiled.*
16. *All stormwater calculations must meet the requirements of the UDC, as well as applicable state and federal regulations, and shall be submitted prior to approval of construction plans by the Engineering and Public Works Departments. Any site layout or building square footage changes necessary to meet these regulations will be reviewed by the City Planner or Development Director to determine whether the project must return to the Planning Commission for approval.*
17. *Any proposed retaining walls shall be submitted for review and approval by Building Codes simultaneously with engineering site plan reviews and must be approved before the issuance of a grading permit.*
18. *Record drawings of the development will be required prior to the issuance of the Certificate of Occupancy (COs) for all installed infrastructure. Final Record Drawing Submittal is required prior to*

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the release of bonds and to include the full design sent in both electronic files and hard copies. The electronic files must be geographically reference to NAD83 and NAVD88 and provided in a format that can be integrated into the City GIS and must meet all City Requirements.

- 19. Long-term maintenance/management plan that meets all UDC requirements, as listed in but not limited to Article 15.7, will be required prior to approval of construction plans by the Engineering Department and before a pre-construction meeting can be held.*
- 20. No sewer and water hookups shall be allowed until corresponding infrastructure is installed, tested, and accepted by the City.*
- 21. The applicant must provide complete and sufficient information in the applicable design, satisfactorily addressing the City Engineer’s comments before any permits or a pre-construction meeting can occur. Any disagreement on information, design, or accuracy of information between the City Engineer and applicant that cannot be resolved must be heard by the Planning Commission for final determination.*
- 22. Any additional requirements as determined during construction and engineering review for the sole purpose of meeting public health, safety, and welfare as determined by the City Engineer, Development Director, Public Works Director, and City Administrator will require field changes or site design revisions as necessary to fully address the concerns. Site layout or building square footage changes required to meet public health, safety, and welfare will be reviewed by the Planning Director or Development Director to determine if the project will be required to go back through Planning Commission approval process. The development representative will hold the right to go before Planning Commission if resolutions to address concerns between staff and the development cannot be agreed upon.*

Site Specific Conditions:

- 1. The applicant shall have six (6) months from site plan approval to execute the proposed lot split. If TDEC Consent Order WPC2025-0093 does not allow for the lot split to occur, then this condition shall be tolled until the closure of Consent Order WPC2025-0093.*
- 2. The multi-use trail along the property shall be completed and constructed per UDC and ADA requirements. The eastern terminus of the multi-use trail shall be the proposed lot split line. In the event the applicant does not move forward with the proposed lot split, then the applicant shall construct the multi-use trail to the eastern property line.*
- 3. The access drive shall have a minimum of 25’ radius for the entrance onto Beechcroft Road.*

Commissioner Connolly seconded the motion. Motion to approve STP 1901-2025 passed by a vote of 6-0.

K. OTHER BUSINESS

L. COMMISSION COMMENT

M. STAFF COMMENT

N. ADJOURN

Chairman Droke adjourned the meeting at 6:01PM.

X _____
Liz Droke, Chairman

X _____
P.C. Secretary

Spring Hill Planning Commission Bond Agenda



TO: Spring Hill Planning Commission
FROM: Missy Stahl, CIP Director
Will Goats, Project Manager
MEETING: January 26, 2026
SUBJECT: Release the maintenance bond and reduce the performance bond for Sanctuary Bluff Apartments.

Background: A maintenance bond was established in the amount of \$133,476.00 for Sanctuary Bluff Apartments in April 2024. The maintenance bond guarantees that certain improvements have been functioning in accordance with plans for a minimum of twelve months, to include water, sewer, stormwater infrastructure, streets, and curbs.

A performance bond was established in the amount of \$262,668.00 for Sanctuary Bluff Apartments in April 2024. The performance bond guarantees installation of improvements to include curbing, sidewalks, street lights, street signs, stormwater surface maintenance, and final asphalt. Final topping was completed in October 2025.

A walk through was conducted and all punch list items have been completed. A certificate of satisfaction has been signed by City staff.

Request:

- Approve Resolution 26-01 to release the maintenance bond for Sanctuary Bluff Apartments
- Approve Resolution 26-02 to release the existing performance bond and establish a maintenance bond for Sanctuary Bluff Apartments

**RESOLUTION 26-01 OF THE
PLANNING COMMISSION
OF THE CITY OF SPRING HILL, TENNESSEE**

**A RESOLUTION TO RELEASE THE EXISTING PERFORMANCE BOND AND
TO ESTABLISH AS A MAINTENANCE BOND FOR
SANCTUARY BLUFF APARTMENTS**

WHEREAS, a Performance Bond is in place guaranteeing the completion of certain improvements for Sanctuary Bluff Apartments in the amount of **\$292,668.00**; and

WHEREAS, the following improvements are required pursuant to the Final Plat:
Curbing, Sidewalks, Street Lights, Street Signs, Stormwater Surface Maintenance, and Final topping to all streets with 1 ½ inches of hot mix asphalt; and

WHEREAS, to date, the improvements have been completed, and approved through inspections by the City and therefore a Maintenance Bond letter of credit is required; and

WHEREAS, a Maintenance Bond letter of credit is guaranteeing the workmanship and materials of certain improvements for Sanctuary Bluff Apartments and the repair of such should damage occur during covered period; and

WHEREAS, it is the recommendation of City staff that the Letter of Credit in the amount of **\$292,668.00** be reduced to 30% according to Section 15.19 of the Spring Hill Unified Development Code, establishing a Maintenance Bond letter of credit in the amount of **\$87,800.00** for a minimum of twelve (12) months from date of work completed.

NOW, THEREFORE BE IT RESOLVED, by the Spring Hill Planning Commission that the existing bond letter of credit be reduced to establish a Maintenance Bond letter of credit for Sanctuary Bluff Apartments in the amount of **\$87,800.00** is hereby approved.

Passed and adopted this 9th day of February, 2026.

Liz Droke, Chairman

Gilles Morin, Secretary

**RESOLUTION 26-02 OF THE
PLANNING COMMISSION
OF THE CITY OF SPRING HILL, TENNESSEE**

**A RESOLUTION TO RELEASE THE MAINTENANCE BOND FOR
SANCTUARY BLUFF APARTMENTS**

WHEREAS, a Maintenance Bond is currently in place for said development; and

WHEREAS, the existing Letter of Credit, considered a “maintenance surety” based on current subdivision regulations, guaranteed that the following improvements, which may include but not be limited to, water, sewer, stormwater infrastructure, street binder; and

WHEREAS, to date, the improvements noted heretofore have been constructed and have been functioning properly for a minimum period of twelve (12) months; and

WHEREAS, it is the recommendation of City staff that the Maintenance Bond for Sanctuary Bluff Apartments in the amount of \$133,476.00 be released.

NOW, THEREFORE BE IT RESOLVED, by the Spring Hill Planning Commission that release of the Maintenance Bond for Sanctuary Bluff Apartments in the amount of **\$133,476.00** is hereby approved.

Passed and adopted this 9th day of February, 2026.

Liz Droke, Chairman

Gilles Morin, Secretary



CERTIFICATE OF SATISFACTORY COMPLETION

Date: 1-21-2026

Nel Bel Sanctuary Bluff

Sanctuary Bluff Apartments

Development Name: Sanctuary Bluff

Phase or Section of Construction: Apartments

Public Improvements: Water, Sewer, Stormwater Infrastructure, Binder, Curbing, Sidewalks, Street lights, Street signs, Stormwater surface maintenance, and final topping to all streets with 1 1/2 inches of hot mix asphalt

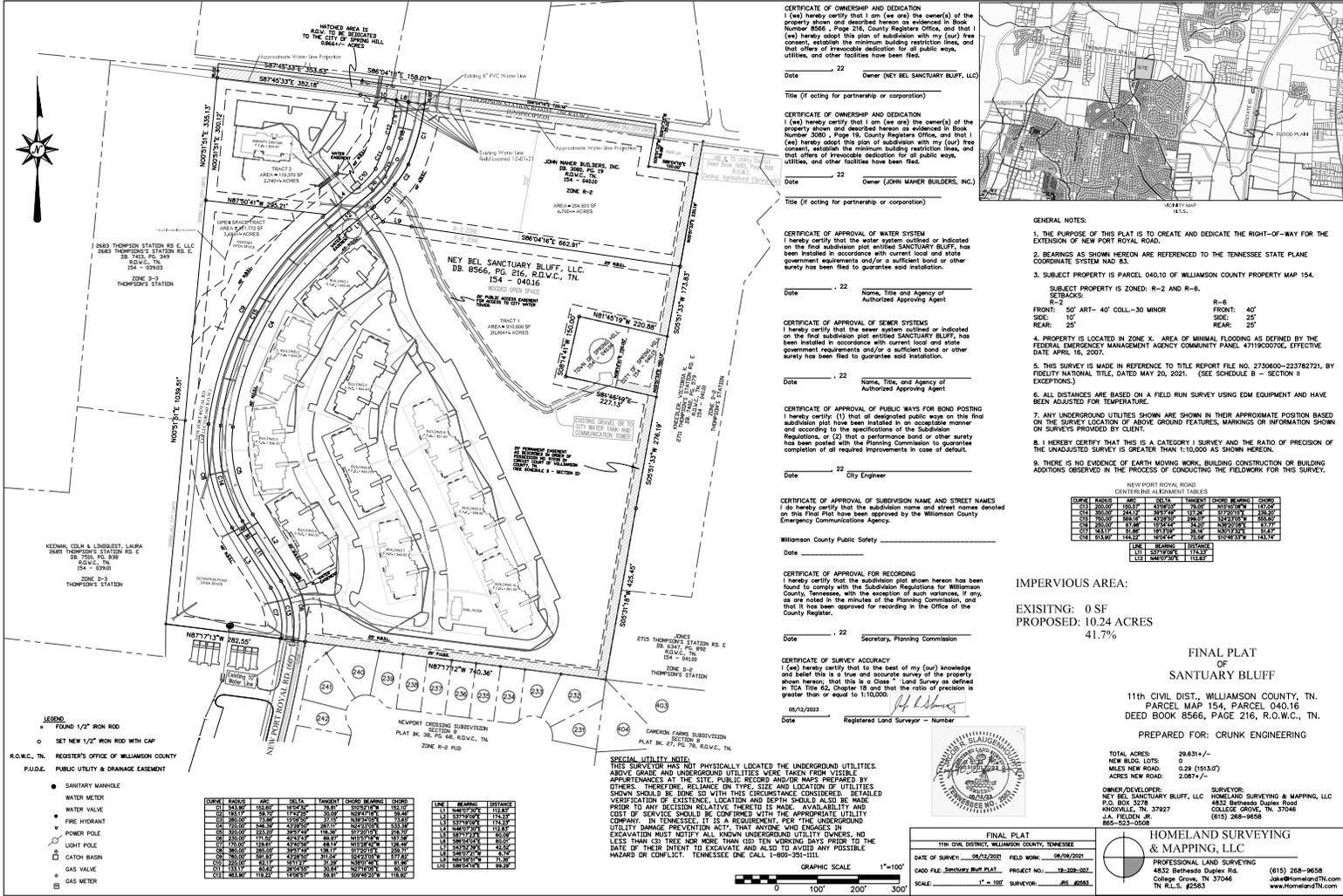
I hereby certify that I have supervised and inspected the improvements to ensure that the design intent has been achieved.

Record Drawings have been submitted by Applicant's engineer to the City pursuant to ordinance requirements.

Approved By:
City of Spring Hill Public Works Director

Tyler Scroggins

199 TOWN CENTER PARKWAY P.O. BOX 789 SPRING HILL, TN 37174
PHONE (931)486-2252 NASHVILLE LINE (615)248-6307 WILLIAMSON CO. (615)599-2614 FAX (931)486-0516



CERTIFICATE OF OWNERSHIP AND DEDICATION
 I (we) hereby certify that I am (we are) the owner(s) of the property shown and described herein as evidenced in Book Number 8566, Page 216, County Registers Office, and that I (we) hereby adopt this plan of subdivision with my (our) free consent, establish the minimum building restriction lines, and that offers of irrevocable dedication for all public ways, utilities, and other facilities have been filed.

Date: 22
 Owner (NEY BEL SANCTUARY BLUFF, LLC)
 Title (if acting for partnership or corporation)

CERTIFICATE OF OWNERSHIP AND DEDICATION
 I (we) hereby certify that I am (we are) the owner(s) of the property shown and described herein as evidenced in Book Number 2080, Page 15, County Registers Office, and that I (we) hereby adopt this plan of subdivision with my (our) free consent, establish the minimum building restriction lines, and that offers of irrevocable dedication for all public ways, utilities, and other facilities have been filed.

Date: 22
 Owner (JOHN MAHER BUILDERS, INC.)
 Title (if acting for partnership or corporation)

CERTIFICATE OF APPROVAL OF WATER SYSTEM
 I hereby certify that the water system outlined or indicated on the first subdivision plat entitled SANCTUARY BLUFF, has been installed in accordance with current local and state government requirements and/or a sufficient bond or other surety has been filed to guarantee said installation.

Date: 22
 Name, Title and Agency of Authorized Approving Agent

CERTIFICATE OF APPROVAL OF SEWER SYSTEMS
 I hereby certify that the sewer system outlined or indicated on the first subdivision plat entitled SANCTUARY BLUFF, has been installed in accordance with current local and state government requirements and/or a sufficient bond or other surety has been filed to guarantee said installation.

Date: 22
 Name, Title, and Agency of Authorized Approving Agent

CERTIFICATE OF APPROVAL OF PUBLIC PLAYS FOR BOND POSTING
 I hereby certify that all designated bondable ways on this final subdivision plat have been installed in accordance with current local and state government requirements and/or a sufficient bond or other surety has been posted with the Planning Commission to guarantee completion of all required improvements in case of default.

Date: 22
 City Engineer

CERTIFICATE OF APPROVAL OF SUBDIVISION NAME AND STREET NAMES
 I do hereby certify that the subdivision name and street names denoted on this Final Plat have been approved by the Williamson County Emergency Communication Agency.

Date: 22
 Secretary, Planning Commission

CERTIFICATE OF SURVEY ACCURACY
 I (we) hereby certify that to the best of my (our) knowledge and belief this is a true and accurate survey of the property shown herein; that this is a Class "A" Land Survey as defined in TCA Title 62, Chapter 18 and that the ratio of precision is greater than or equal to 1:10,000.

Date: 05/22/2023
 Registered Land Surveyor - Number



- GENERAL NOTES:**
- THE PURPOSE OF THIS PLAT IS TO CREATE AND DEDICATE THE RIGHT-OF-WAY FOR THE EXTENSION OF NEWPORT ROYAL ROAD.
 - BEARINGS AS SHOWN HEREON ARE REFERENCED TO THE TENNESSEE STATE PLANE COORDINATE SYSTEM NAD 83.
 - SUBJECT PROPERTY IS PARCEL 040.10 OF WILLIAMSON COUNTY PROPERTY MAP 154.
 - PROPERTY IS LOCATED IN ZONE X. AREA OF MINIMAL FLOODING AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY COMMUNITY PANEL 4711800076, EFFECTIVE DATE APRIL 16, 2007.
 - THIS SURVEY IS MADE IN REFERENCE TO TITLE REPORT FILE NO. 273000-22378271, BY FREELTY NATIONAL TITLE, DATED MAY 20, 2021. (SEE SCHEDULE B - SECTION II EXCEPTIONS).
 - ALL DISTANCES ARE BASED ON A FIELD RUN SURVEY USING EDM EQUIPMENT AND HAVE BEEN ADJUSTED FOR TEMPERATURE.
 - ANY UNDERGROUND UTILITIES SHOWN ARE SHOWN IN THEIR APPROXIMATE POSITION BASED ON THE SURVEY LOCATION OF ABOVE GROUND FEATURES, MARKINGS OR INFORMATION SHOWN ON SURVEYS PROVIDED BY CLIENT.
 - I HEREBY CERTIFY THAT THIS IS A CATEGORY I SURVEY AND THE RATIO OF PRECISION OF THE UNADJUSTED SURVEY IS GREATER THAN 1:10,000 AS SHOWN HEREON.
 - THERE IS NO EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS OBSERVED IN THE PROCESS OF CONDUCTING THE FIELDWORK FOR THIS SURVEY.

NEWPORT ROYAL ROAD CENTERLINE ALIGNMENT TABLES

LINE	BEARING	DISTANCE
1	N 87° 50' 41" W	245.61'
2	S 88° 04' 16" E	158.01'
3	S 87° 50' 41" W	245.61'
4	N 87° 50' 41" W	245.61'
5	S 88° 04' 16" E	158.01'
6	N 87° 50' 41" W	245.61'
7	S 88° 04' 16" E	158.01'
8	N 87° 50' 41" W	245.61'
9	S 88° 04' 16" E	158.01'
10	N 87° 50' 41" W	245.61'
11	S 88° 04' 16" E	158.01'
12	N 87° 50' 41" W	245.61'
13	S 88° 04' 16" E	158.01'
14	N 87° 50' 41" W	245.61'
15	S 88° 04' 16" E	158.01'
16	N 87° 50' 41" W	245.61'
17	S 88° 04' 16" E	158.01'
18	N 87° 50' 41" W	245.61'
19	S 88° 04' 16" E	158.01'
20	N 87° 50' 41" W	245.61'
21	S 88° 04' 16" E	158.01'
22	N 87° 50' 41" W	245.61'
23	S 88° 04' 16" E	158.01'
24	N 87° 50' 41" W	245.61'
25	S 88° 04' 16" E	158.01'
26	N 87° 50' 41" W	245.61'
27	S 88° 04' 16" E	158.01'
28	N 87° 50' 41" W	245.61'
29	S 88° 04' 16" E	158.01'
30	N 87° 50' 41" W	245.61'
31	S 88° 04' 16" E	158.01'
32	N 87° 50' 41" W	245.61'
33	S 88° 04' 16" E	158.01'
34	N 87° 50' 41" W	245.61'
35	S 88° 04' 16" E	158.01'
36	N 87° 50' 41" W	245.61'
37	S 88° 04' 16" E	158.01'
38	N 87° 50' 41" W	245.61'
39	S 88° 04' 16" E	158.01'
40	N 87° 50' 41" W	245.61'
41	S 88° 04' 16" E	158.01'
42	N 87° 50' 41" W	245.61'
43	S 88° 04' 16" E	158.01'
44	N 87° 50' 41" W	245.61'
45	S 88° 04' 16" E	158.01'
46	N 87° 50' 41" W	245.61'
47	S 88° 04' 16" E	158.01'
48	N 87° 50' 41" W	245.61'
49	S 88° 04' 16" E	158.01'
50	N 87° 50' 41" W	245.61'
51	S 88° 04' 16" E	158.01'
52	N 87° 50' 41" W	245.61'
53	S 88° 04' 16" E	158.01'
54	N 87° 50' 41" W	245.61'
55	S 88° 04' 16" E	158.01'
56	N 87° 50' 41" W	245.61'
57	S 88° 04' 16" E	158.01'
58	N 87° 50' 41" W	245.61'
59	S 88° 04' 16" E	158.01'
60	N 87° 50' 41" W	245.61'
61	S 88° 04' 16" E	158.01'
62	N 87° 50' 41" W	245.61'
63	S 88° 04' 16" E	158.01'
64	N 87° 50' 41" W	245.61'
65	S 88° 04' 16" E	158.01'
66	N 87° 50' 41" W	245.61'
67	S 88° 04' 16" E	158.01'
68	N 87° 50' 41" W	245.61'
69	S 88° 04' 16" E	158.01'
70	N 87° 50' 41" W	245.61'
71	S 88° 04' 16" E	158.01'
72	N 87° 50' 41" W	245.61'
73	S 88° 04' 16" E	158.01'
74	N 87° 50' 41" W	245.61'
75	S 88° 04' 16" E	158.01'
76	N 87° 50' 41" W	245.61'
77	S 88° 04' 16" E	158.01'
78	N 87° 50' 41" W	245.61'
79	S 88° 04' 16" E	158.01'
80	N 87° 50' 41" W	245.61'
81	S 88° 04' 16" E	158.01'
82	N 87° 50' 41" W	245.61'
83	S 88° 04' 16" E	158.01'
84	N 87° 50' 41" W	245.61'
85	S 88° 04' 16" E	158.01'
86	N 87° 50' 41" W	245.61'
87	S 88° 04' 16" E	158.01'
88	N 87° 50' 41" W	245.61'
89	S 88° 04' 16" E	158.01'
90	N 87° 50' 41" W	245.61'
91	S 88° 04' 16" E	158.01'
92	N 87° 50' 41" W	245.61'
93	S 88° 04' 16" E	158.01'
94	N 87° 50' 41" W	245.61'
95	S 88° 04' 16" E	158.01'
96	N 87° 50' 41" W	245.61'
97	S 88° 04' 16" E	158.01'
98	N 87° 50' 41" W	245.61'
99	S 88° 04' 16" E	158.01'
100	N 87° 50' 41" W	245.61'

IMPERVIOUS AREA:
 EXISTING: 0 SF
 PROPOSED: 10.24 ACRES
 41.7%

FINAL PLAT
OF
SANCTUARY BLUFF
 11th CIVIL DIST., WILLIAMSON COUNTY, TN.
 PARCEL MAP 154, PARCEL 040.16
 DEED BOOK 8566, PAGE 216, R.O.W.C., TN.
 PREPARED FOR: CRUNK ENGINEERING

- LEGEND**
- FOUND 1/2" IRON ROD
 - SET NEW 1/2" IRON ROD WITH CAP
 - R.O.W.C., TN. REGISTER'S OFFICE OF WILLIAMSON COUNTY
 - P.U.D.E. PUBLIC UTILITY & DRAINAGE EASEMENT
 - SANITARY MANHOLE
 - WATER METER
 - WATER VALVE
 - FIRE HYDRANT
 - POWER POLE
 - LIGHT POLE
 - CATCH BASIN
 - GAS VALVE
 - GAS METER

DATE	NO.	ARC	DELTA	ANGLE	CHORD	BEARING	CHORD
01	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
02	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
03	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
04	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
05	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
06	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
07	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
08	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
09	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
10	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
11	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
12	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
13	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
14	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
15	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
16	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
17	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
18	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
19	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
20	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
21	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
22	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
23	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
24	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
25	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
26	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
27	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
28	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
29	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
30	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
31	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
32	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
33	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
34	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
35	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
36	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
37	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
38	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
39	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
40	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
41	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
42	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
43	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
44	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
45	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
46	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
47	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
48	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
49	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
50	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
51	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
52	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
53	28.50	132.00	0.0000	76.80	80.0000	132.00	28.50
54	28.50	132.00	0.0000	76.80	80.0000	132.00	28.5

Spring Hill Planning Commission Bond Agenda



TO: Spring Hill Planning Commission
FROM: Missy Stahl, CIP Director
Will Goats, Project Manager
MEETING: January 26, 2026
SUBJECT: Release the maintenance bond and recommend acceptance of dedication of road right-of-way and public improvements for Barton Hills Phase 1A

Background: A maintenance bond was established in the amount of \$97,010.76 for Barton Hills Phase 1A in October 2024. The maintenance bond guarantees that certain improvements installed have been functioning in accordance with plans for a minimum of twelve months, to include sidewalks, street signs, streetlights, stormwater surface maintenance, and final topping. Final topping was installed in August 2024.

A walk through was conducted and all punch list items have been completed. A certificate of satisfaction has been signed by City staff.

Request:

- Approve Resolution 26-03 to release the maintenance bond for Barton Hills Phase 1A
- Approve Resolution 26-04 to recommend acceptance and dedication of road right-of-way and public improvements for Barton Hills Phase 1A

**RESOLUTION 26-03 OF THE
PLANNING COMMISSION
OF THE CITY OF SPRING HILL, TENNESSEE**

**A RESOLUTION TO RELEASE THE MAINTENANCE BOND FOR
BARTON HILLS PHASE 1A**

WHEREAS, a Maintenance Bond is currently in place for said development; and

WHEREAS, the existing Letter of Credit, considered a “maintenance surety” based on current subdivision regulations, guaranteed that the following improvements, which may include but not be limited to, sidewalks, street signs, street lights, stormwater surface maintenance, and final topping to all streets with 1 ½ inches of hot mix asphalt; and

WHEREAS, to date, the improvements noted heretofore have been constructed and have been functioning properly for a minimum period of twelve (12) months; and

WHEREAS, release of said maintenance bond shall be contingent upon the Board of Mayor and Alderman approving a resolution for acceptance and dedication of road right-of-way and public improvements for said development; and

WHEREAS, it is the recommendation of City staff that the Maintenance Bond for Barton Hills Phase 1A in the amount of \$97,010.76 be released.

NOW, THEREFORE BE IT RESOLVED, by the Spring Hill Planning Commission that release of the Maintenance Bond for Barton Hills Phase 1A in the amount of **\$97,010.76** is hereby approved.

Passed and adopted this 9th day of February, 2026.

Liz Droke, Chairman

Gilles Morin, Secretary

**RESOLUTION 26-04 OF THE
PLANNING COMMISSION
OF THE CITY OF SPRING HILL, TENNESSEE**

**A RESOLUTION RECOMMENDING ACCEPTANCE AND DEDICATION OF ROAD
RIGHTS-OF-WAY AND PUBLIC IMPROVEMENTS SHOWN ON THE EXISTING
PLAT FOR
BARTON HILLS PHASE 1A**

WHEREAS, Copperstone Development Partners, LLC has a recorded Final Plat for Barton Hills Phase 1A in Maury County Plat Book P22, Page 132; and

WHEREAS, said Plat show Public Rights-of-Way proposed for dedication to the City of Spring Hill; and

WHEREAS, an Offer of Dedication, Deeds of Conveyance and a Maintenance Surety have been submitted pursuant to the Subdivision Regulations; and

WHEREAS, a Certificate of Satisfactory Completion has been furnished by the City of Spring Hill indicating that through inspections of the Road Rights-of-Way, the design intent has been achieved; and

WHEREAS, the developer is required under Article III, Section 6.3 of the Subdivision Regulations to submit an “as-built” survey of the public improvements including water, sewer and drainage; and

WHEREAS, the Planning Commission deems it in the best interest of the City of Spring Hill that the Offer of Dedication of the Road Rights-of-Way and Public Improvement’s be accepted and the same become a part of the Public Street system of the City of Spring Hill.

NOW, THEREFORE BE IT RESOLVED, by the Spring Hill Planning Commission that dedication and acceptance of Road Rights-of-Way and Public Improvements within Barton Hills Phase 1A as shown on the recorded plats is hereby recommended to the Board of Mayor and Aldermen.

Passed and adopted this 9th day of February, 2026.

Liz Droke, Chairman

Gilles Morin, Secretary



CERTIFICATE OF SATISFACTORY COMPLETION

Date: 1-21-2026

Copperstone Development Partners, LLC
Barton Hills
Phase 1A

Development Name: Barton Hills

Phase or Section of Construction: Phase 1A

Public Improvements: Sidewalks, Street Lights, Street Signs, Stormwater Surface Maintenance, and Final Topping to all streets with 1 1/2 inches of hot mix asphalt

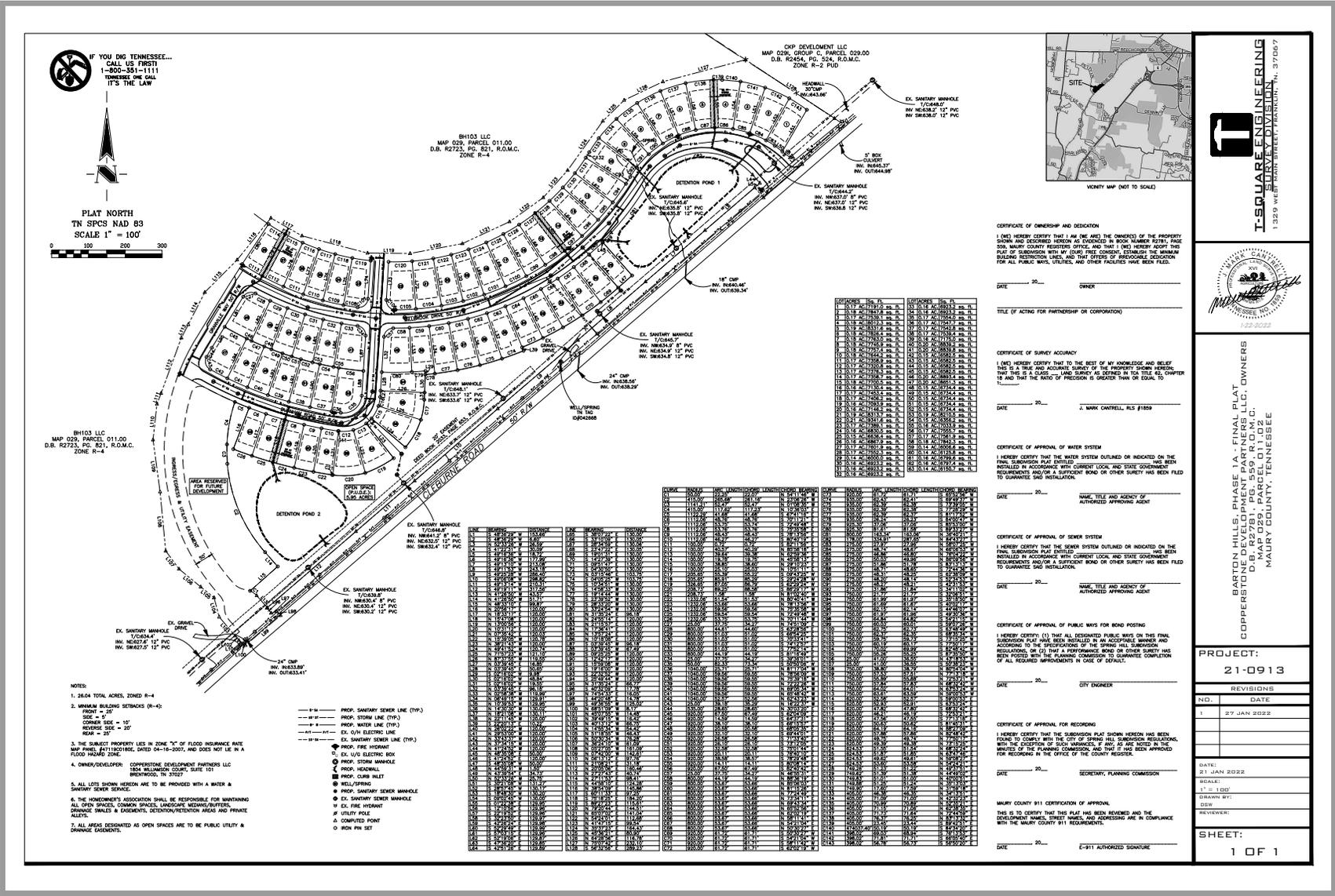
I hereby certify that I have supervised and inspected the improvements to ensure that the design intent has been achieved.

Record Drawings have been submitted by Applicant's engineer to the City pursuant to ordinance requirements.

Approved By:
City of Spring Hill Public Works Director

Helen Scarpino

199 TOWN CENTER PARKWAY P.O. BOX 789 SPRING HILL, TN 37174
PHONE (931)486-2252 NASHVILLE LINE (615)248-6307 WILLIAMSON CO. (615)599-2614 FAX (931)486-0516



Spring Hill Planning Commission Bond Agenda



TO: Spring Hill Planning Commission
FROM: Missy Stahl, CIP Director
Will Goats, Project Manager
MEETING: January 26, 2026
SUBJECT: Reduce the performance bond for Brandon Woods Traffic Signal

Background: A performance bond was established in the amount of \$250,000.00 for Brandon Woods Traffic Signal in April 2021. The performance bond guarantees installation of improvements to include installation of a traffic signal at the intersection of Brandon Woods Drive and Port Royal Road. The traffic signal was installed in November 2025.

A walk through was conducted and all punch list items have been completed. A certificate of satisfaction has been signed by City staff.

Request:

- Approve Resolution 26-05 to release the existing performance bond and establish a maintenance bond for Brandon Woods Traffic Signal

**RESOLUTION 26-05 OF THE
PLANNING COMMISSION
OF THE CITY OF SPRING HILL, TENNESSEE**

**A RESOLUTION TO RELEASE THE EXISTING PERFORMANCE BOND AND
TO ESTABLISH AS A MAINTENANCE BOND FOR
BRANDON WOODS TRAFFIC SIGNAL**

WHEREAS, a Performance Bond is in place guaranteeing the completion of certain improvements for Brandon Woods Traffic Signal in the amount of **\$250,000.00**; and

WHEREAS, the following improvements are required pursuant to the Final Plat:
Installation of a traffic signal at the intersection of Brandon Woods Drive and Port Royal Road; and

WHEREAS, to date, the improvements have been completed, and approved through inspections by the City and therefore a Maintenance Bond letter of credit is required; and

WHEREAS, a Maintenance Bond letter of credit is guaranteeing the workmanship and materials of certain improvements for Brandon Woods Traffic Signal and the repair of such should damage occur during covered period; and

WHEREAS, it is the recommendation of City staff that the Letter of Credit in the amount of **\$250,000.00** be reduced to 30% according to Section 15.19 of the Spring Hill Unified Development Code, establishing a Maintenance Bond letter of credit in the amount of **\$75,000.00** for a minimum of twelve (12) months from date of work completed.

NOW, THEREFORE BE IT RESOLVED, by the Spring Hill Planning Commission that the existing bond letter of credit be reduced to establish a Maintenance Bond letter of credit for Brandon Woods Traffic Signal in the amount of **\$75,000.00** is hereby approved.

Passed and adopted this 9th day of February, 2026.

Liz Droke, Chairman

Gilles Morin, Secretary



CERTIFICATE OF SATISFACTORY COMPLETION

Date: 1-23-2026

Brandon Woods, LLC
Brandon Woods
Traffic Signal

Development Name: Brandon Woods

Phase or Section of Construction: Traffic Signal

Public Improvements: Installation of a traffic signal at the intersection of Brandon Woods Drive and Port Royal Road

I hereby certify that I have supervised and inspected the improvements to ensure that the design intent has been achieved.

Record Drawings have been submitted by Applicant's engineer to the City pursuant to ordinance requirements.

Approved By:
City of Spring Hill Public Works Director



199 TOWN CENTER PARKWAY P.O. BOX 789 SPRING HILL, TN 37174
PHONE (931)486-2252 NASHVILLE LINE (615)248-6307 WILLIAMSON CO. (615)599-2614 FAX (931)486-0516

Spring Hill Planning Commission Bond Agenda



TO: Spring Hill Planning Commission
FROM: Missy Stahl, CIP Director
Will Goats, Project Manager
MEETING: January 26, 2026
SUBJECT: Establish a performance bond for Legacy Pointe Phase 1A

Background: The developer is installing public infrastructure that needs to be bonded. This development was approved on the February 2024 PC Agenda (PDF 1524-2023). Developer has submitted the required documentation for the application of the bonds.

Request:

- Approve Resolution 26-06 to establish a performance bond for Legacy Pointe Phase 1A

**RESOLUTION 26-06 OF THE
PLANNING COMMISSION
OF THE CITY OF SPRING HILL, TENNESSEE**

**A RESOLUTION TO ESTABLISH A PERFORMANCE BOND FOR
LEGACY POINTE PHASE 1A**

WHEREAS, a Performance Bond is required to be established for this development prior to recording of a Final Plat; and

WHEREAS, the Performance Bond is guaranteeing the construction of certain improvements on 116 lots; and

WHEREAS, the following improvements are required pursuant to the Final Plat:
*Sewer, Water, Stormwater Infrastructure, Binder, Curbing, Sidewalks,
Stormwater Surface Maintenance; and*

WHEREAS, to date, the improvements have not been completed and/or accepted by the City and, therefore, a Performance Bond is required; and

WHEREAS, it is the recommendation of the City staff that a Performance Bond be established in the amount of **\$5,304,000.00**; and

WHEREAS, it is anticipated that the date of completion for the above referenced public improvements will be within the time prescribed for the bond and it is required that an automatic renewal clause, to the benefit of the City of Spring Hill, be included within the bond in case such improvements are not completed in a timely manner; and

WHEREAS, upon completion of the public improvements listed above, the Developer will be required to file a “maintenance” bond guaranteeing performance of the public improvements for a minimum of an additional one-year period with the Planning Commission before the dedication and acceptance of such public improvements by the Board of Mayor and Aldermen.

NOW, THEREFORE, BE IT RESOLVED, that the Spring Hill Planning Commission approves the establishment of a Performance Bond for Legacy Pointe Phase 1A in the amount of **\$5,304,000.00**.

Passed and adopted this 9th day of February, 2026.

Liz Droke, Chairman

Gilles Morin, Secretary



Utility Information Sheet - Performance Bond

Development	Legacy Pointe		
	Phase	Section	Number of Lots
	1A	1	116

Cost to Install:	
1. Sewer Line Cost:	\$ 1,325,988.35
Lenth of line (LF)	1371 of 8"
Type of pipe used	SDR 26
Backfill material	Crushed Stone
Depth of sewer line	Varies
Number of manholes	13
Number of services	44
Number of fittings	159
Number of clean out boxes	115
2. Water Line Cost:	\$ 925,611.18
Lenth of line (LF)	1844 (116 of 6", 1728 of 8")
Type of pipe used	Ductile Iron Pipe
Backfill material	Crushed Stone
Number of meters	110 meters (22 fire 88 regular)
Size of meters	22 2" and 88 3/4" meters
Number of gate valves	11
Size of gate valves	5 6" and 6 8"
Number of fire hydrants	5
3. Stormwater Infrastructure Cost:	\$ 447,202.27
Length of line (LF)	2552
Pipe size	Varies (18" @ 2353" and 24" @ 199)
Type of pipe	RCP
Number of headwalls	
Number of curb inlets	6
Number of area drains	15
Number of outfalls	2
Number of retention pond(s)	
Number of detention pond(s)	1
4. Binder Cost:	\$ 667,769.87
Road length (LF)	1800'
Road width (LF)	25'
Classification of road	Local (Private)
	(Collector, local, arterial)
5. Signage Cost:	\$ -
Number of signs	
	(stop signs, street names, speed limits, etc.)
6. Street Lights Cost:	
Number of street lights	
7. Curbing Cost:	\$ 20,268.10
Curb length (LF)	841
Type	Mostly curb and gutter
	(curb & gutter or extruded)
8. Sidewalks Cost:	\$ 15,000.00
Sidewalk Length (LF)	200

Number of ADA ramps	16
9. Final Asphalt Cost:	\$ -
Road length (LF)	
Road width (LF)	
Classification of road	
(Collector, local, arterial)	
10. Stormwater Surface Maintenance Cost:	\$ 134,160.68
(to include drainage area, ditches, retention and/or detention ponds, open spaces)	
**30% of stormwater infrastructure cost (refer to maintenance bond cost)	
TOTAL COST:	\$ 3,536,000.45
Developer Signature	Date
DUSTIN WALCOTT <small>DESIGNED BY DUSTIN WALCOTT DR #1274 DUSTIN WALCOTT@ASHFORDWOODS.COM O-ASHFORD 1000 SOUTHWEST HERRICK O-LAND DEVELOPMENT, O-0101010 WALCOTT 04/20/2018 11:22:20 18 10-08507</small>	
FOR INTERNAL USE ONLY:	
Total amount of bond:	\$5,304,000.68
Cost verified by staff: <i>[Signature]</i>	
	City Signature

City of Spring Hill
 199 Town Center Parkway
 P.O. Box 789



Spring Hill, TN 37174
 Phone 931-486-2252 Ext. 488
 Fax: 931-486-3596

Application for Surety

Property Information	Project Name: Legacy Pointe Phase 1A Phase: Phase 1A Section: N/A Number of Lots Approved: 116 Number of Lots Remaining: N/A Surety Type: <input type="checkbox"/> Maintenance <input checked="" type="checkbox"/> Performance <input type="checkbox"/> Restoration Posted With: <input checked="" type="checkbox"/> Letter of Credit <input type="checkbox"/> Performance Bond <input checked="" type="checkbox"/> Insurance Bond <input type="checkbox"/> Cash Surety Amount: \$ _____ Expiration Date: ____/____/____ Automatic Renewal Clause included with Surety: <input checked="" type="checkbox"/> Yes / <input type="checkbox"/> No (Circle One) Purpose of Surety: Legacy Pointe Phase 1A Proposed Roadway
	Name of Financial Institution: HUB International Midwest (Agency) Surety #: N/A Contact Person: Stasha King Email: stasha.king@hubinternational.com Address: 1411 Opus Place, Ste. 450 City, State, Zip: Downers Grove, IL 60515 Phone Number: (630) 468-5678 Fax Number : (____) _____
Contact Information	Name of Owner/Developer or Representative: Ashton Woods c/o Ralph Knauss Address: 370 Mallory Station Road, Suite 500 City, State, Zip: Franklin, TN 37069 Email: rknauss@southstarco.com PhoneNumber: (615) 796-8676 Fax Number: (____) _____

Action Request

I (we) request that the following action be taken:

- Establish New Surety
 - Request Final Inspection and Release of Surety
 - Request Reduction of Surety Amount
 - Request extension of surety for (1) year
 - Request Maintenance Bond
- (Please provide proof of difficulty below)

Explanation for Proof of Difficulty:

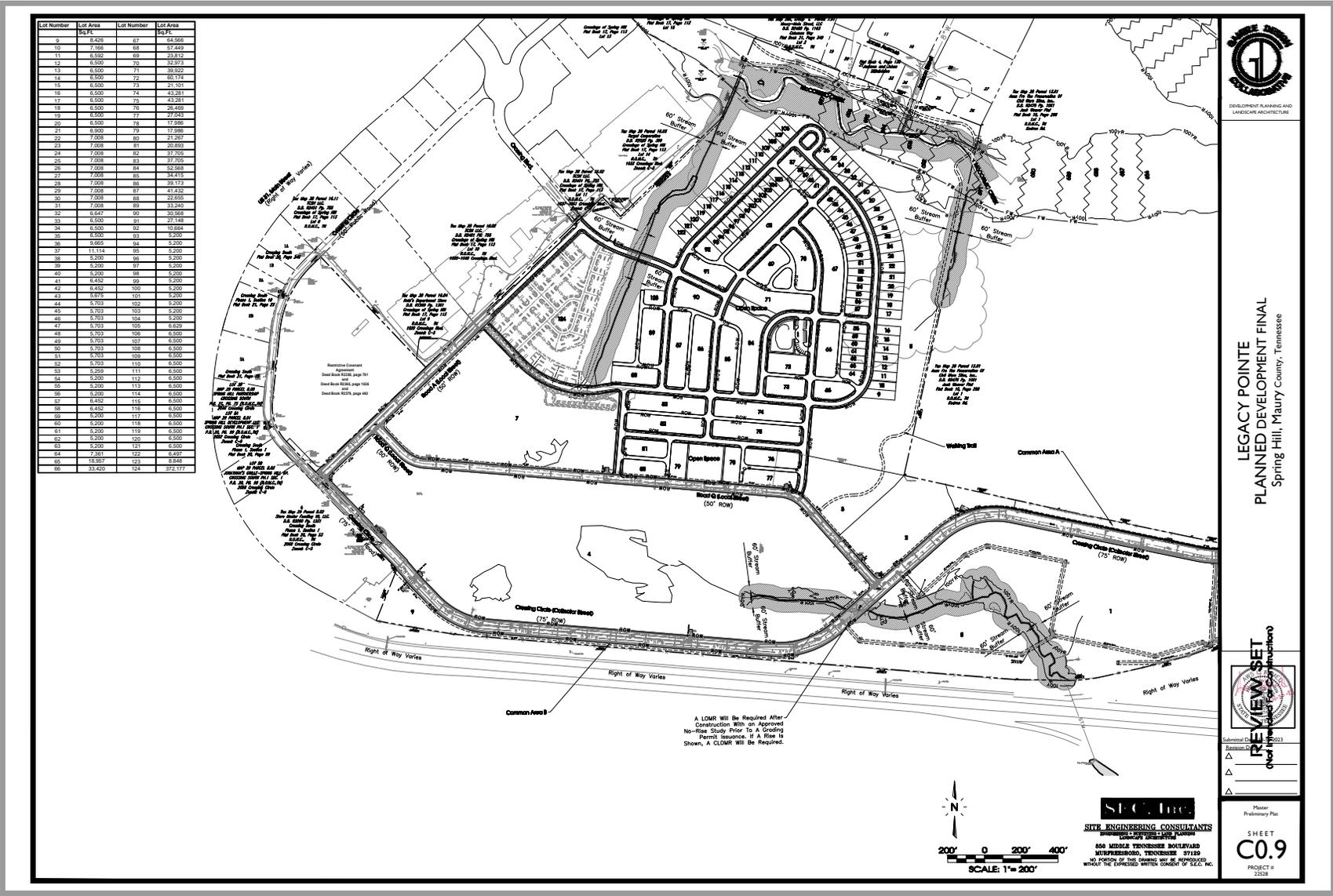
Chet Bachula Digitally signed by Chet Bachula
 DN: cn=US,
 e=chet.bachula@ashtonwoods.com,
 o=Ashton Woods, OU=Land
 Development, CN=Chet Bachula
 Date: 2025.12.12 12:23:49-0600'

Applicant Signature

Date

City Staff Signature

Date



Spring Hill Planning Commission Work Session



TO: Spring Hill Planning Commission
 FROM: Kali-Flynn Burke, Associate Planner
 MEETING: January 26, 2026
 SUBJECT: STP 1965-2025 (Platinum Storage)

STP 1965-2025: Submitted by Kimley Horn for **Platinum Storage**. The property is located at 830 Beechcroft Road, located on the north side of Beechcroft Road, east of Dr. Robertson Road. It is zoned I1, Light Industrial, and is approximately 7.67 acres.

Request: The applicant is requesting site plan approval for the expansion of the existing storage facility, which includes climate controlled, non-climate controlled, covered RV, and boat storage.

Property Description and History: The property is located on the north side of Beechcroft Road, east of Dr. Robertson Road. The site is relatively flat and has been mostly clear of trees. There is a small stream, Walden Branch, that receives water and drains to the southwest of the site. The original site plan, STP 669-2019, was approved for an approximately 89,604 square foot self-storage facility by Planning Commission on March 9, 2020. A final plat, FPM 1327-2022, for a lot split was approved on December 12, 2022. A revision, SPM 1471-2023, was approved on December 11, 2023, to increase the usage of the property by adding 63 mobile storage units and recreational vehicle parking. There are two sections of the site that have been constructed and are operating.

Zoning, Bulk, and Area Requirements: The setback requirements are shown in the table below:

Building Setbacks (I-1)	Required	Provided	Meets Requirements
Front	30'	30'	Yes
Interior Side	20'	N/A*	Unknown
Corner Side	30'	30'	Yes
Rear	10'	10'	Yes

*Setbacks are not applicable.

Access and Site Circulation: This site has one ingress/egress point which is accessible from Beechcroft Road, and it runs north/south. The proposed addition would utilize the existing ingress/egress and extend it across Walden Branch to a portion of the site that is currently undeveloped.

Streets and Sidewalks: The property has frontage on both Beechcroft Road and Dr. Robertson Road. There is a multi-use trail along Beechcroft Road. Dr. Robertson and Beechcroft Road are designated as Arterial Streets on the Major Thoroughfare Plan (MTP). The UDC establishes a minimum 95' Right-of-Way dimension for Arterial Streets. The applicant shall show the 47.5' future right-of-way line from the centerline of both streets for revision deadline #2, and all setbacks and required improvements shall be measured from the future right-of-way.

Parking: The parking requirements are shown in the table below:

	Use/Requirement		Required	Provided	Meets Requirements
Auto Spaces	1 per 20 storage units: enclosed (154 units)	1 per 25 storage units: outdoor (82 units)	11	11	Yes
Bicycle Spaces	1 per 20 spaces		1	None	No*

*Please show the location of the proposed bicycle parking.

The applicant will need to dimension drive aisles and parking spaces to confirm compliance.

Landscape: The applicant has provided a landscape plan. However, UDC Article 11.4 A.4 states that shrubs are to be 30” in height and three out of their four shrubs are only 18” in height. The undersized shrubs shall be corrected to meet UDC requirements for revision deadline #2.

Design Standards and Materials: The site plan shows 6 buildings with 236 total units. The proposed veneer consists of an earthy palette of a red brick veneer, two different grey brick veneers, and cast stone coping. This building is the same material and color as the previously built buildings on site.

Design Standards for the I1 district are shown in the table below:

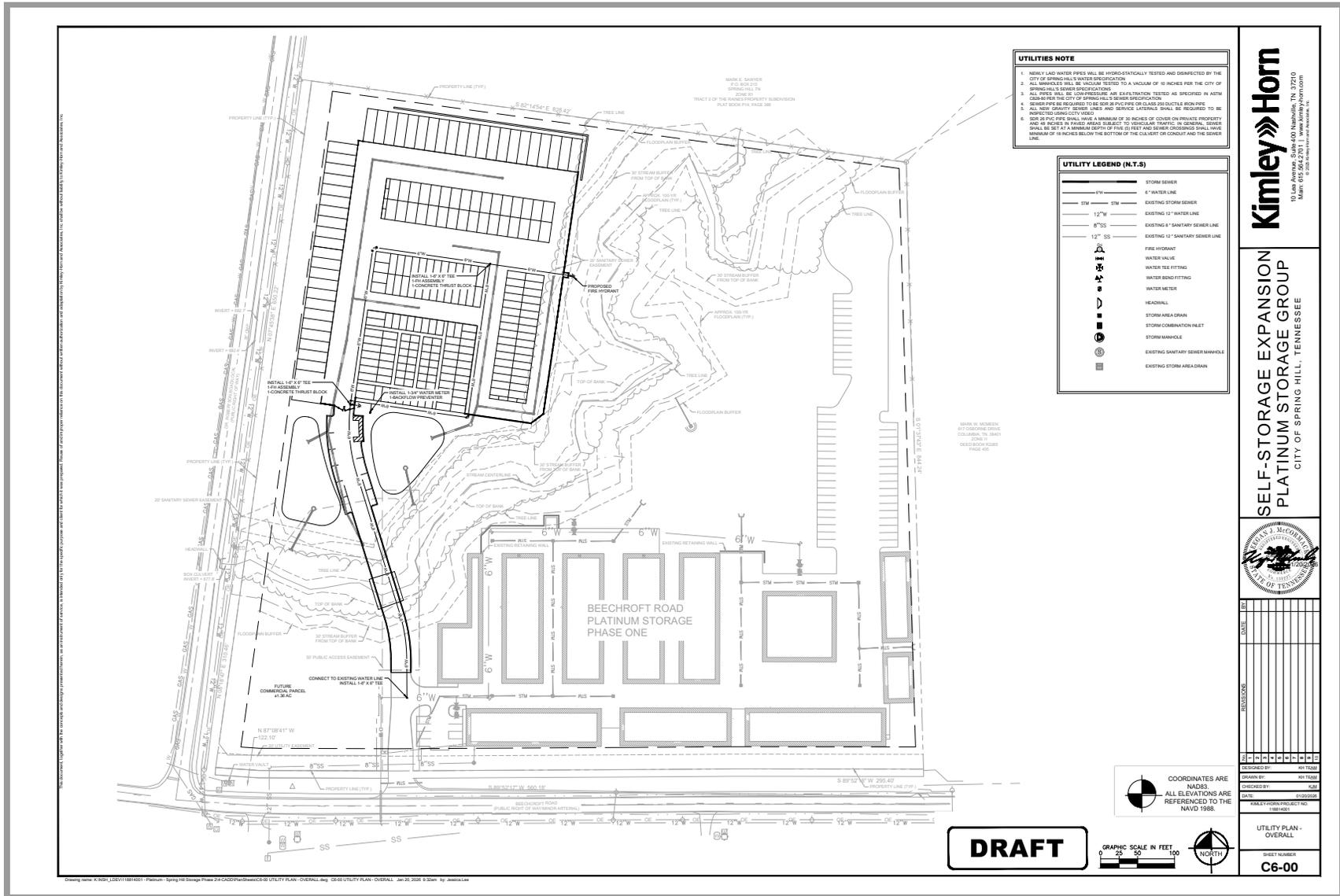
Standard	Meets Requirement
<u>Facade Design</u>	
Building façades that abut a public right-of-way, excluding alleys, must not contain blank wall areas that exceed 35 linear feet, measured parallel to the street.	Unknown
<u>Roof Design</u>	
Mechanical equipment on roofs must be screened from view from all sides by the use of parapet walls or enclosures designed to conceal the equipment as required by this Code.	Unknown
<u>Entrance Design</u>	
Applicant has met I1 design standards.	Yes
<u>Site Design</u>	
Applicant has met I1 design standards.	Yes
<u>Site Elements</u>	
Applicant has met I1 design standards.	Yes

The design standards meet the UDC requirements with variations in façade height, color, and adequate transparency. However, the allowable percentages of materials cannot be confirmed. The applicant shall provide material percentages for revision deadline #2 to demonstrate compliance with the City’s design standards.

Discussion: The proposed building A on the west side of the property has a blank wall. There is variation in color and height, however, staff would like Planning Commission feedback on the blank wall (west side) facing a public right-of-way.

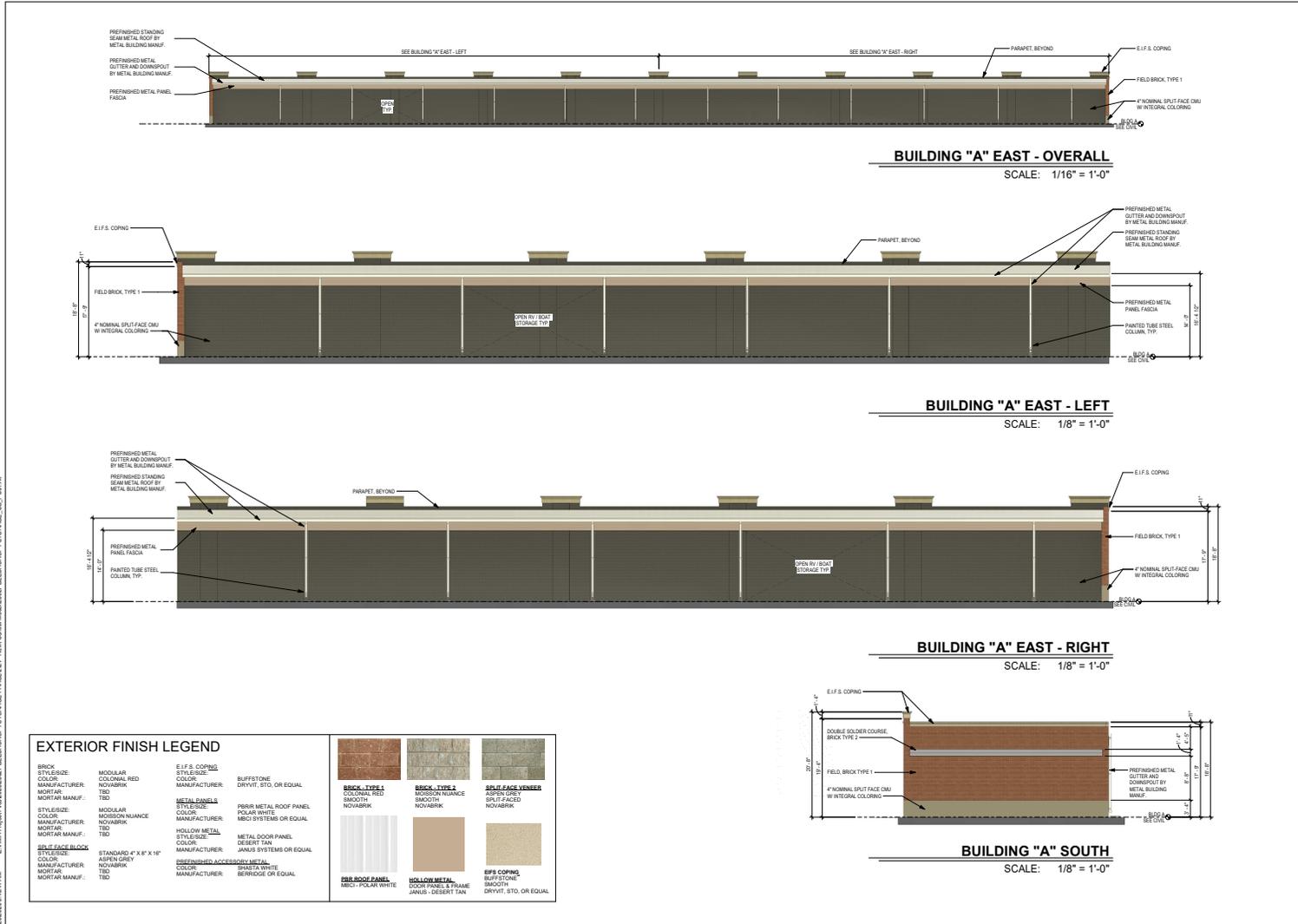
Summary: The applicant met with staff to discuss this application on January 14, 2026. There are several items below that require revisions and/or further discussion prior to the Voting Meeting:

- Adjust shrubs on the landscape plan to be at least 30” in height.
- Provide at least one bike rack in accordance with Article 10.4.A of the UDC.
- Show the number of units in the two previous phases in addition to this phase.
- Provide a percentage of impervious surface for the whole site.
- Dimension the parking spaces from inside of white line to inside of white line.
- Dimension the width of the parking lot and drive aisles.
- Please correct the striping for the ADA parking spot.
- Provide details regarding drainage of the HVAC system providing climate-controlled environment.
- Please provide the dimensions of the right-of-way from centerline for both Dr. Robertson Road and Beechcroft Road.
- Please dimension the setbacks from the future right-of-way line, and measure all required distances from that line.
- Show the existing turn lanes along Beechcroft Road.
- Provide building material breakdown percentages.
- Show a cross-access easement for the future commercial parcel development on the southwest corner of the property. (Article 10.6 C)
- Provide mechanical equipment and screening with the next submittal. It cannot be addressed in the building permit submittals.



H M H A
 H. Michael Hindman
 Architects, P.C.

1001 Peachtree Circle, Suite 100
 Marietta, Georgia 30067
 770.429.9100
 www.hmha.com



EXTERIOR FINISH LEGEND

<p>BRICK STYLE/SIZE: MODULAR COLOR: COLONIAL RED MANUFACTURER: NOVARRK MORTAR MANUF.: TBD</p>	<p>E.I.F.'S COPING STYLE/SIZE: MODULAR COLOR: COLONIAL RED MANUFACTURER: NOVARRK MORTAR MANUF.: TBD</p>	<p>BUFFSTONE STYLE/SIZE: BUFF/VT, STD. OR EQUAL COLOR: BUFFSTONE MANUFACTURER: MARIETTA</p>	<p>BRICK - TYPE 1 COLONIAL RED SMOOTH NOVARRK</p>	<p>BRICK - TYPE 2 MOISSON NUANCE SMOOTH NOVARRK</p>	<p>SPLIT-FACE VENEER ASPEN GREY SPLIT-FACED NOVARRK</p>
<p>METAL PANELS STYLE/SIZE: MODULAR COLOR: MOISSON NUANCE MANUFACTURER: NOVARRK MORTAR MANUF.: TBD</p>	<p>PRE-METAL ROOF PANEL STYLE/SIZE: POLAR WHITE COLOR: POLAR WHITE MANUFACTURER: MECI SYSTEMS OR EQUAL</p>	<p>HOLLOW METAL STYLE/SIZE: METAL DOOR PANEL COLOR: DESERT TAN MANUFACTURER: JANUS SYSTEMS OR EQUAL</p>	<p>PRE ROOF PANEL MECI-POLAR WHITE</p>	<p>HOLLOW METAL DOOR PANEL & FRAME JANUS - DESERT TAN</p>	<p>E.I.F.'S COPING BUFFSTONE DRYVT, STD. OR EQUAL</p>

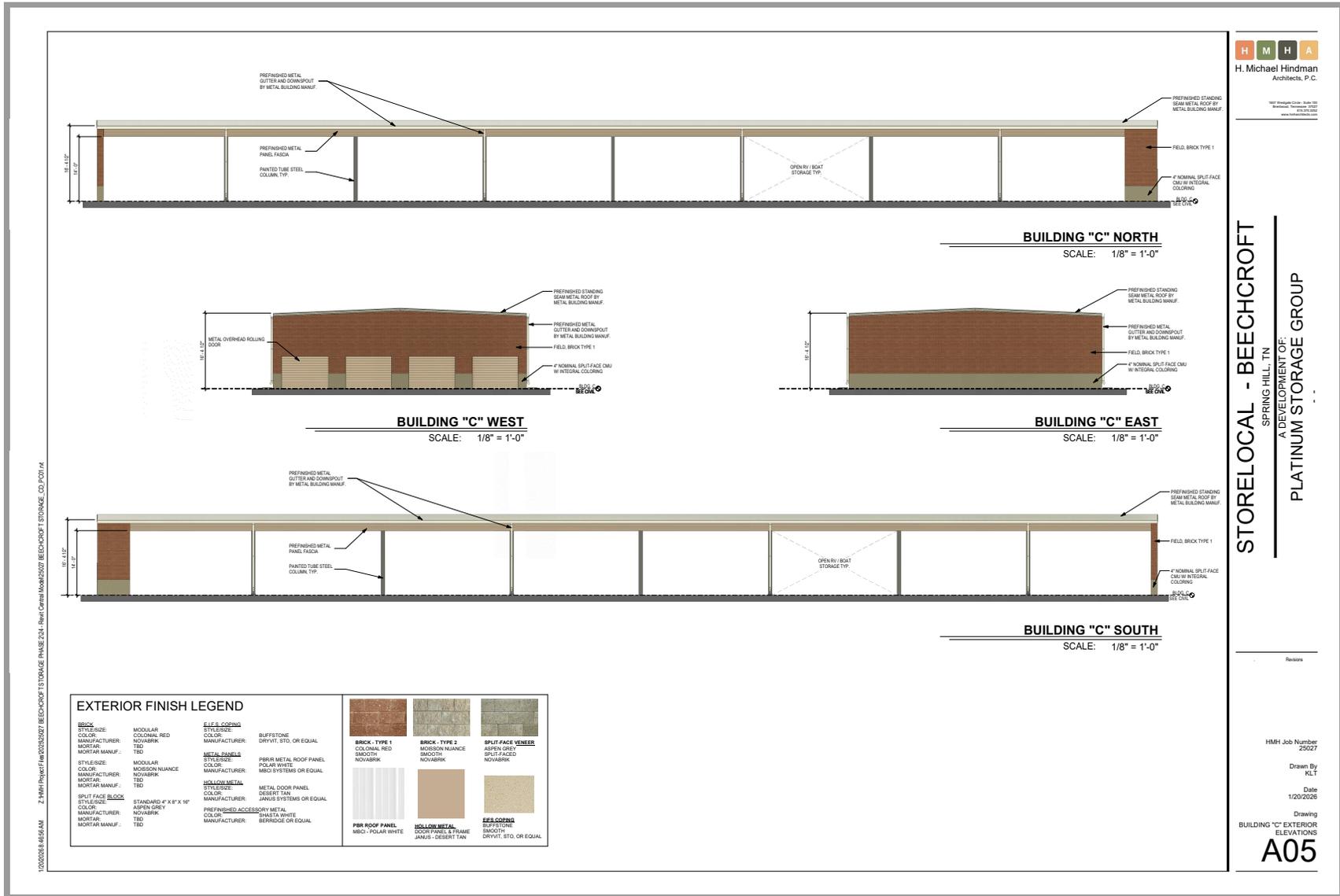
STORELOCAL - BEECHCROFT
 SPRING HILL, TN
 A DEVELOPMENT OF:
PLATINUM STORAGE GROUP

HMH Job Number
 25027

Drawn By
 KLT

Date
 1/20/2026

Drawing
 BUILDING "A" EXTERIOR ELEVATIONS
A02



H M H A
H. Michael Hindman
Architects, P.C.

STORELOCAL - BEECHCROFT
SPRING HILL, TN
A DEVELOPMENT OF:
PLATINUM STORAGE GROUP

HMH Job Number
25027
Drawn By
KLT
Date
1/20/2026
Drawing
BUILDING "C" EXTERIOR
ELEVATIONS
A05

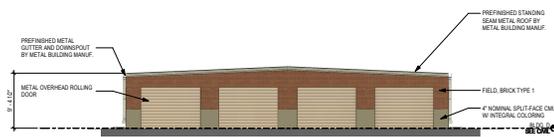


H. Michael Hindman
Architects, P.C.

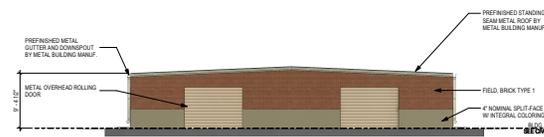
1001 Peachtree Circle, Suite 1000
Marietta, Georgia 30067
(770) 575-1000
www.hmh.com



BUILDING "D" NORTH
SCALE: 1/8" = 1'-0"



BUILDING "D" WEST
SCALE: 1/8" = 1'-0"



BUILDING "D" EAST
SCALE: 1/8" = 1'-0"



BUILDING "D" SOUTH
SCALE: 1/8" = 1'-0"

EXTERIOR FINISH LEGEND

BRICK STYLE/SIZE: COLOR: MANUFACTURER: MORTAR MANUF.:	MODULAR COLOR: MANUFACTURER: MORTAR MANUF.:	E.I.F.S. COPING STYLE/SIZE: COLOR: MANUFACTURER:	BUFFSTONE DRYVIT, STO, OR EQUAL:
STYLE/SIZE COLOR: MANUFACTURER: MORTAR MANUF.:	MODULAR COLOR: MANUFACTURER: MORTAR MANUF.:	METAL PANELS STYLE/SIZE: COLOR: MANUFACTURER:	PBR METAL ROOF PANEL COLOR: MANUFACTURER: MORTAR MANUF.:
SPLIT FACE BLOCK STYLE/SIZE: COLOR: MANUFACTURER: MORTAR MANUF.:	STANDARD 4" X 8" X 16" COLOR: MANUFACTURER: MORTAR MANUF.:	HOLLOW METAL STYLE/SIZE: COLOR: MANUFACTURER:	METAL DOOR PANEL COLOR: MANUFACTURER: MORTAR MANUF.:
		PREFINISHED ACCESSORY METAL COLOR: MANUFACTURER:	



STORELOCAL - BEECHCROFT
SPRING HILL, TN
A DEVELOPMENT OF:
PLATINUM STORAGE GROUP

Revisions

HMH Job Number
25027

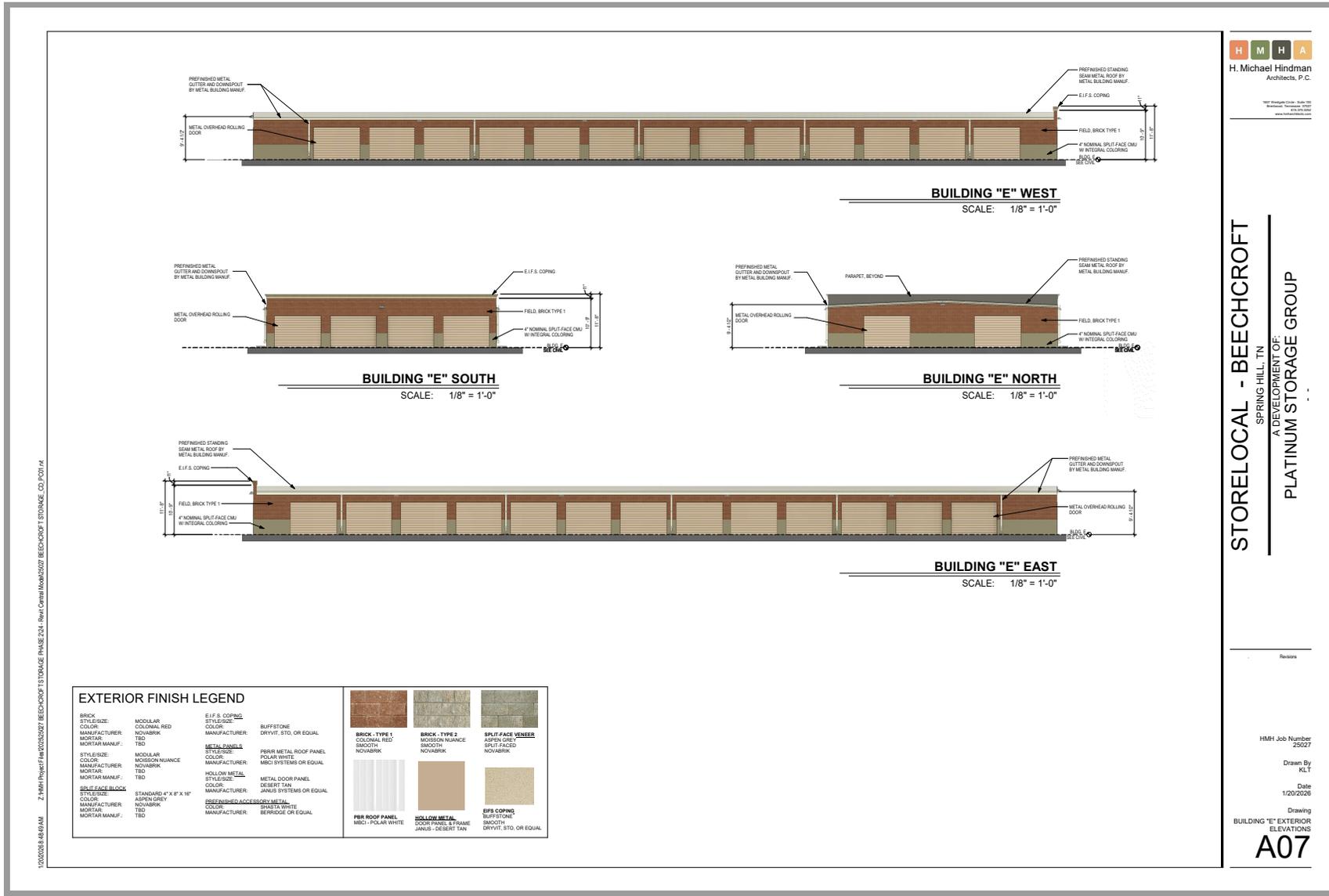
Drawn By
KLT

Date
1/20/2026

Drawing
BUILDING "D" EXTERIOR
ELEVATIONS

A06

1/20/2026 8:47 AM Z:\HM\Project\Final\20260207_BEECHCROFT_STORAGE_PHASE2\1 - Rev1\Drawings\25027_BEECHCROFT_STORAGE_CO.PLOT.rvt



H M H A
 H. Michael Hindman
 Architects, P.C.
1001 Peachtree City, Suite 100
 Peachtree City, GA 30269
 770.287.1100

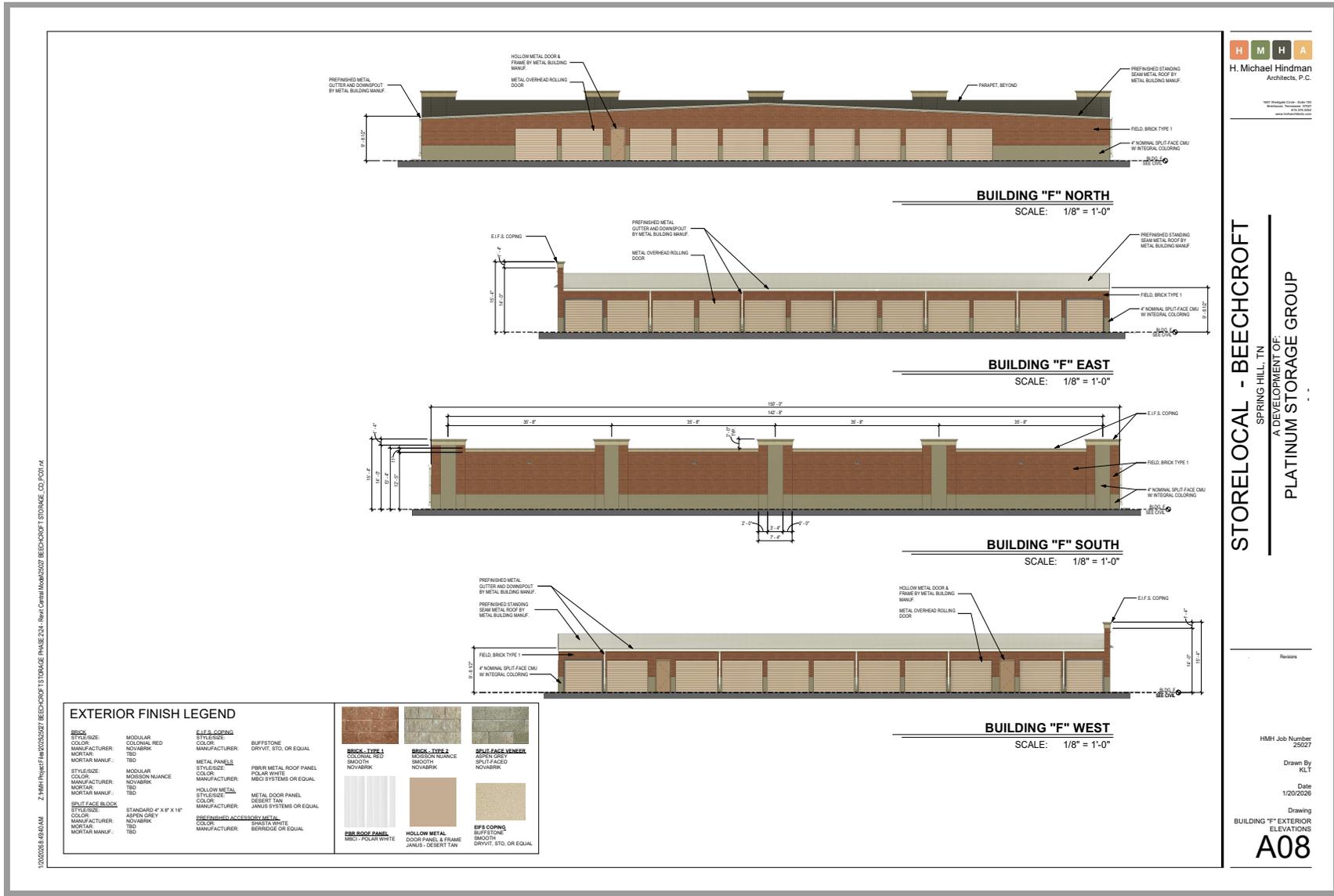
STORELOCAL - BEECHCROFT
 SPRING HILL, TN
 A DEVELOPMENT OF:
PLATINUM STORAGE GROUP

EXTERIOR FINISH LEGEND

BRICK STYLE/SIZE: COLOR: MANUFACTURER: MORTAR: MORTAR MANUF.:	MODULAR COLOR: COLONIAL RED MANUFACTURER: NEW BRICK	EIFS COPING STYLE/SIZE: COLOR:	BUFFSTONE DRYVT, STD. OR EQUAL		BRICK - TYPE 1 COLOR: COLONIAL RED STYLE/SIZE: MANUFACTURER:		BRICK - TYPE 2 COLOR: MOISSON NUANCE STYLE/SIZE: MANUFACTURER:		SPLIT FACE VENEER COLOR: ASPEN GREY STYLE/SIZE: MANUFACTURER:
METAL PANELS STYLE/SIZE: COLOR: MANUFACTURER: MORTAR: MORTAR MANUF.:	COLLAR COLOR: MOISSON NUANCE MANUFACTURER: NOV BRICK	HOLLOW METAL STYLE/SIZE: COLOR: MANUFACTURER:	PRR METAL ROOF PANEL COLOR: POLAR WHITE MANUFACTURER: MBI SYSTEMS OR EQUAL		PRR ROOF PANEL COLOR: MBI - POLAR WHITE		HOLLOW METAL DOOR PANEL COLOR: JANUS MANUFACTURER: S.T. NAME		EIFS COPING STYLE/SIZE: COLOR: MANUFACTURER:
SELF FACE BLOCK STYLE/SIZE: COLOR: MANUFACTURER: MORTAR: MORTAR MANUF.:	STANDARD 4" X 8" X 16" COLOR: ASPEN GREY MANUFACTURER: NOV BRICK	SMOOTH METAL STYLE/SIZE: COLOR: MANUFACTURER:	METAL DOOR PANEL COLOR: DESERT TAN MANUFACTURER: JANUS SYSTEMS OR EQUAL		SMOOTH METAL STYLE/SIZE: COLOR: MANUFACTURER:		EIFS COPING STYLE/SIZE: COLOR: MANUFACTURER:		EIFS COPING STYLE/SIZE: COLOR: MANUFACTURER:

Revisions

HMH Job Number: 25027
 Drawn By: KLT
 Date: 1/20/2026
 Drawing: BUILDING "E" EXTERIOR ELEVATIONS
A07



H M H A
H. Michael Hindman
Architects, P.C.
1000 Peachtree Circle, Suite 1000
Atlanta, Georgia 30309
404.525.1000

STORELOCAL - BEECHCROFT
SPRING HILL, TN
A DEVELOPMENT OF:
PLATINUM STORAGE GROUP

Revisions
HMH Job Number: 25027
Drawn By: KLT
Date: 1/20/2026
Drawing: BUILDING "F" EXTERIOR ELEVATIONS
A08



January 13, 2026

Mr. Gilles Morin, AICP
City of Spring Hill
Development Services
8060 Station Hill Dr.
Spring Hill, TN 37174

RE: *Certification of Zero Sewer Demand – Platinum Storage*

Mr. Morin,

On behalf of the referenced project, this letter serves as a professional engineering certification regarding sanitary sewer demand associated with the proposed self-storage expansion.

I hereby certify that the proposed expansion will generate zero (0) additional sanitary sewer demand. The project does not include any increase in plumbing fixtures, change in use, or any other modification that would result in additional wastewater generation beyond current conditions.

Any proposed water service associated with this expansion is limited solely to fire protection purposes. No domestic water service is proposed, and no connection to the sanitary sewer system is required as part of this project.

Based on my review, the existing sanitary sewer demand will remain unchanged as a result of this proposed expansion.

Please feel free to contact me if additional information or clarification is required.

Sincerely,



Keegan McCormack, PE
Tennessee Professional Engineer No. 130227
615.200.8972
keegan.mccormack@kimley-horn.com

kimley-horn.com

10 Lea Avenue, Suite 400, Nashville, TN 37210

615 564 2701

Spring Hill Planning Commission Work Session



TO: Spring Hill Planning Commission
FROM: Gilles Morin, Senior Planner
MEETING: January 26, 2026
SUBJECT: ADM 1973-2025 (2981 Stewart Campbell Pointe - Easement Vacation)

ADM 1973-2025: Submitted by Andrew Kozlowitz for 2981 Stewart Campbell Pointe. The property is located on the south side of Stewart Campbell Pointe, east of Kidman Lane, known as Lot 237 of the Cherry Grove Addition Subdivision, Phase 5. The property is zoned R-2, Single-Family District, and is approximately 0.249 acres.

Request: The applicant requests the City to vacate a portion of the drainage easement along the rear (southern portion) of the property from 20 feet to 10 feet. The Planning Commission's role in this request is to forward a recommendation to the Board of Mayor and Alderman (BOMA), as the easement has already been dedicated to the City and may only be vacated or reduced by BOMA action.

Property description and history: The property is located in the Cherry Grove neighborhood and is developed with a single-family home. The property owner submitted a building permit to install a swimming pool and deck at the rear of the property. Upon review of the information submitted by the property owner, staff found that the pool was proposed to be constructed outside the existing drainage easement. A permit for the pool was subsequently issued. The purpose of this easement is to protect the path of stormwater flows during significant rain events and to provide the City with adequate access to maintain or repair existing drainage structures supporting the stormwater network for the entire neighborhood. However, after further research staff discovered the information submitted by the applicant labeled a 10' Drainage Easement when it was actually 20'. The Codes Division stopped work on the proposed pool until an evaluation of the entire neighborhood was conducted related to drainage. Staff concluded the applicant could finish installation of the pool. It was agreed the applicant would submit this application to reduce the Drainage Easement.

Discussion: Development Services staff coordinated with Public Works staff to evaluate the potential for reducing the drainage easement on the subject property in order to create a path forward for the property owner to continue to install the swimming pool. Public Works staff determined that, due to another drainage easement dedicated on the adjoining properties to the south, the drainage easement on the subject property could be reduced to 10 feet without creating stormwater or maintenance access issues.

There are no known negative impacts to the neighborhood's drainage system at this point. Staff will continue to monitor the situation and update as needed.

Summary: The applicant met with staff to discuss this application on January 14, 2026. There were no additional items that need to be addressed.

Andrew Kozlowitz
2981 Stewart Cambell Pt
Spring Hill, TN 37174

RE: Letter of Request of PUDE Abandonment (Adjustment)

To whom it may concern:

This letter is to serve as my request for a PUDE adjustment in the rear of our lot to 10 feet. Per Williamson County, our rear PUDE is 20'.

On June 7, 2025 our swimming pool contractor requested a plot plan from the City permit office. The City sent a non-surveyed plot plan indicating 10' PUDE in the rear. Our contractor drafted plans to accommodate the City's Unified Development Code and adhered to regulations. Additionally, these plans were stamped by a licensed surveyor, sent to the City for approval and for the request of issuance of a building permit for the pool.

The City approved the plans provided, and issued a permit. After construction began, it was determined by the City that the actual PUDE was 20' and not 10' as originally indicated or confirmed by the surveyor. At this time the pool was installed and plumbing/electrical was completed. A stop order was issued, and work halted for 30 days. On September 19, the City issued a resume order on the work so we could restore our property and complete hardscapes around the pool.

We are requesting a PUDE allowance be made in the rear of our property to 10' from the 20' originally indicated by the County so that we are in non-violation.

If we had known that the PUDE was 20' from the start of the project despite the plot plan provided and the surveyor confirming (incorrectly), we would have not installed the pool.

We need an allowance of this abandonment to be in compliance, as we have followed the process and worked closely with the City's officials to resolve this matter.

Sincerely,

Andrew Kozlowitz

Proof of Ownership

This Instrument Prepared by and Return to:
Bell & Alexander Title Services, Inc.
8110 Isabella Lane, 2nd Floor
Brentwood, TN 37027

STATE OF TENNESSEE
COUNTY OF Williamson

Actual consideration or value, whichever is greater is \$940,000.00.



Affiant _____

Subscribed and sworn before me this the
8th day of May, 2025.

Notary Public
My Commission expires: 9/20/25

WARRANTY DEED

Address of New Owner:	Send Tax Bill to:	Tax Information:
Andrew S. Kozlowitz	<u>Neighborhood Mortgage, Inc</u>	166A F 009.00
2981 Stewart Campbell Point	<u>1835 Lockberry Dr. Ste 306</u>	
Spring Hill, TN 37174	<u>Alpharetta, GA 30004</u>	

For and in consideration of the sum of ten and no/100 dollars (\$10.00), cash in hand paid by the hereinafter Grantees, and other good and valuable consideration, the receipt of which is hereby acknowledged, JEREMY HOLDERFIELD AND ANNIE HOLDERFIELD TRUSTEES OF THE HOLDERFIELD LIVING TRUST DATED NOVEMBER 29, 2022 AND THOMAS UNDERWOOD, AND JANICE UNDERWOOD, HUSBAND AND WIFE, hereinafter referred to as Grantors, have bargained and sold, and do hereby transfer and convey to ANDREW S. KOZLOWITZ AND ASHLEY M. KOZLOWITZ, HUSBAND AND WIFE, hereinafter called the Grantees, their successors and assigns, a certain tract or parcel of land, together with all tenements, hereditaments and appurtenances thereto located in Williamson County, State of Tennessee, described as follows:

Land in Williamson County, Tennessee, being Lot No. 237, Phase Five, Cherry Grove Addition, of record in Plat Book P64, page 94, in the Register's Office for Williamson County, Tennessee, to which Plan reference is hereby made for a more complete description of the property.

Being the same property conveyed to Jeremy Holderfield and Annie Holderfield, Trustees of the Holderfield Living Trust dated November 29, 2022, by deed dated 11/29/2022, from Jeremy Holderfield and Annie Holderfield, husband and wife, of record in Book 9162, page 281, Register's Office for said County. (as to their 50% interest)

Also being the same property conveyed to Jeremy Holderfield and Annie Holderfield, husband and wife, AND Thomas Underwood and Janice Underwood, husband and wife, by deed dated 03/28/2017, from Firm Foundation Custom Homes LLC, a Tennessee limited liability company, of record in Book 7037, page 895, Register's Office for said County.

Subject to all Easements and Restrictions of record.

THIS IS IMPROVED PROPERTY KNOWN AS 2981 Stewart Campbell Point, Spring Hill, TN 37174.

To have and to hold the said tract or parcel of land, with the appurtenances, estate, title and interest thereto belonging to the said Grantees, their heirs, successors and/or assigns, forever. Grantors covenant with Grantees that Grantors are lawfully seized and possessed of said land in fee simple, have a good right to convey it, and the same is unencumbered, unless otherwise herein set out. Grantors do further covenant and bind ourselves, our heirs, successors, assigns and representatives, to warrant and forever defend the title to said land to said Grantees, their successors, heirs and assigns, against the lawful claims of all persons whomsoever. Wherever used, the singular number shall include the plural, the plural the singular, and the use of any gender shall be applicable to all genders.

Witness my hand this the 8th day of May, 2025.

[Signature]
Jeremy Holderfield, Trustee

[Signature]
Annie Holderfield, Trustee

STATE OF Tennessee
COUNTY OF Williamson



Before me, the undersigned, a Notary Public in and for the State and County aforesaid, personally appeared Jeremy Holderfield and Annie Holderfield, with whom I am personally acquainted, or proved to me on the basis of satisfactory evidence, and who upon their oath acknowledged themselves to be the trustees of the within named bargainor, a Trust, and that they, as such trustee, executed the foregoing instrument for the purposes therein contained by signing the name of the Trust by the said Jeremy Holderfield and Annie Holderfield as such trustee.

Witness my hand and official seal this 8th day of May, 2025.

My Commission Expires:

7/17/28

[Signature]
Notary Public

[Signature]
Thomas Underwood

[Signature]
Janice Underwood

STATE OF TENNESSEE
COUNTY OF WILLIAMSON

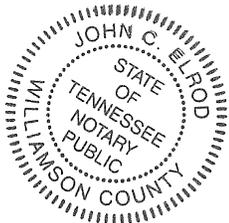
Personally appeared before me, the undersigned, a Notary Public in and for said county and state, Thomas Underwood and Janice Underwood, with whom I am personally acquainted, or proved to me on the basis of satisfactory evidence, and who acknowledged they executed the within instrument for the purposes therein contained.

Witness my hand and official seal this 8th day of May, 2025.

My commission expires:

7/17/28

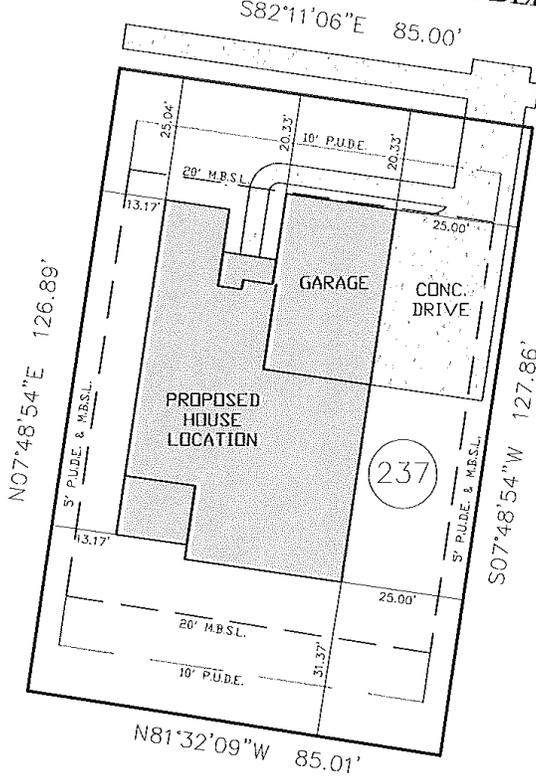
[Signature]
Notary Public



Site plan provided by City 6/9/25



STEWART CAMPBELL PT. (60')



238

236

237

NOTES:

- 1. BEARINGS SHOWN HEREON ARE BASED THE RECORD PLAT OF PHASE 5, CHERRY GROVE ADDITION.
- 2. THIS IS NOT A GENERAL PROPERTY SURVEY AS DESCRIBED IN THE TENNESSEE RULES AND REGULATIONS FOR LAND SURVEYORS.

LEGEND:

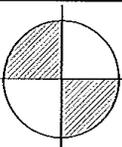
- P.U.D.E. PUBLIC UTILITY & DRAINAGE EASEMENT
- M.B.S.L. MINIMUM BUILDING SETBACK LINE
- R.O.W.C., TN. REGISTERS OFFICE OF WILLIAMSON CNTY, TN.

PLOT PLAN FOR LOT 237

PHASE 5 CHERRY GROVE ADDITION

PLAT BOOK 64, PAGE 94, R.O.W.C. TN. CITY OF SPRING HILL, WILLIAMSON COUNTY, TN.

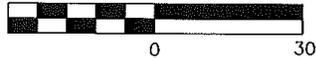
Prepared for: FIRM FOUNDATION CUSTOM HOMES



HOMELAND SURVEYING

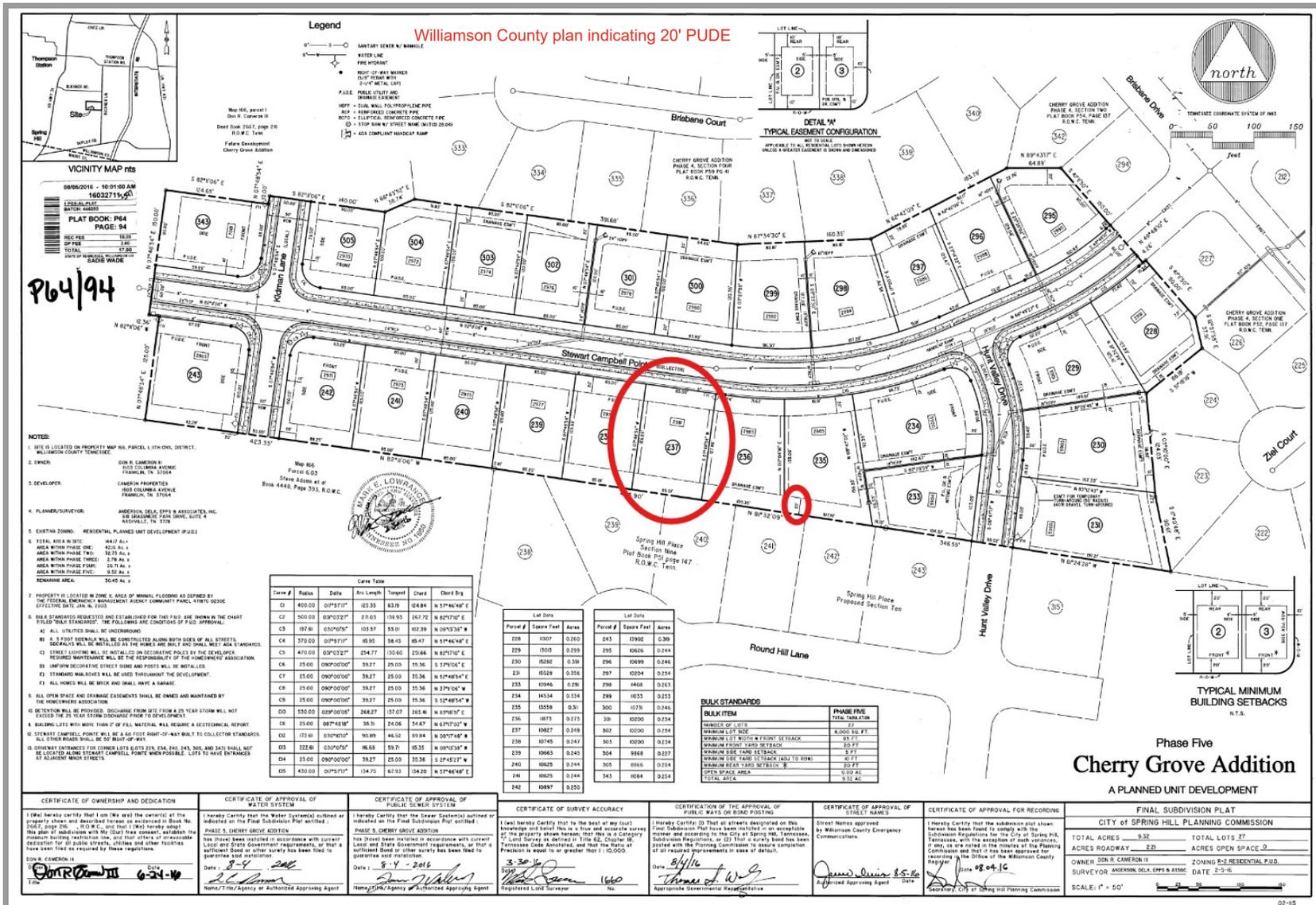
PROFESSIONAL LAND SURVEYING (615) 268-9658 Jakerls2583@gmail.com

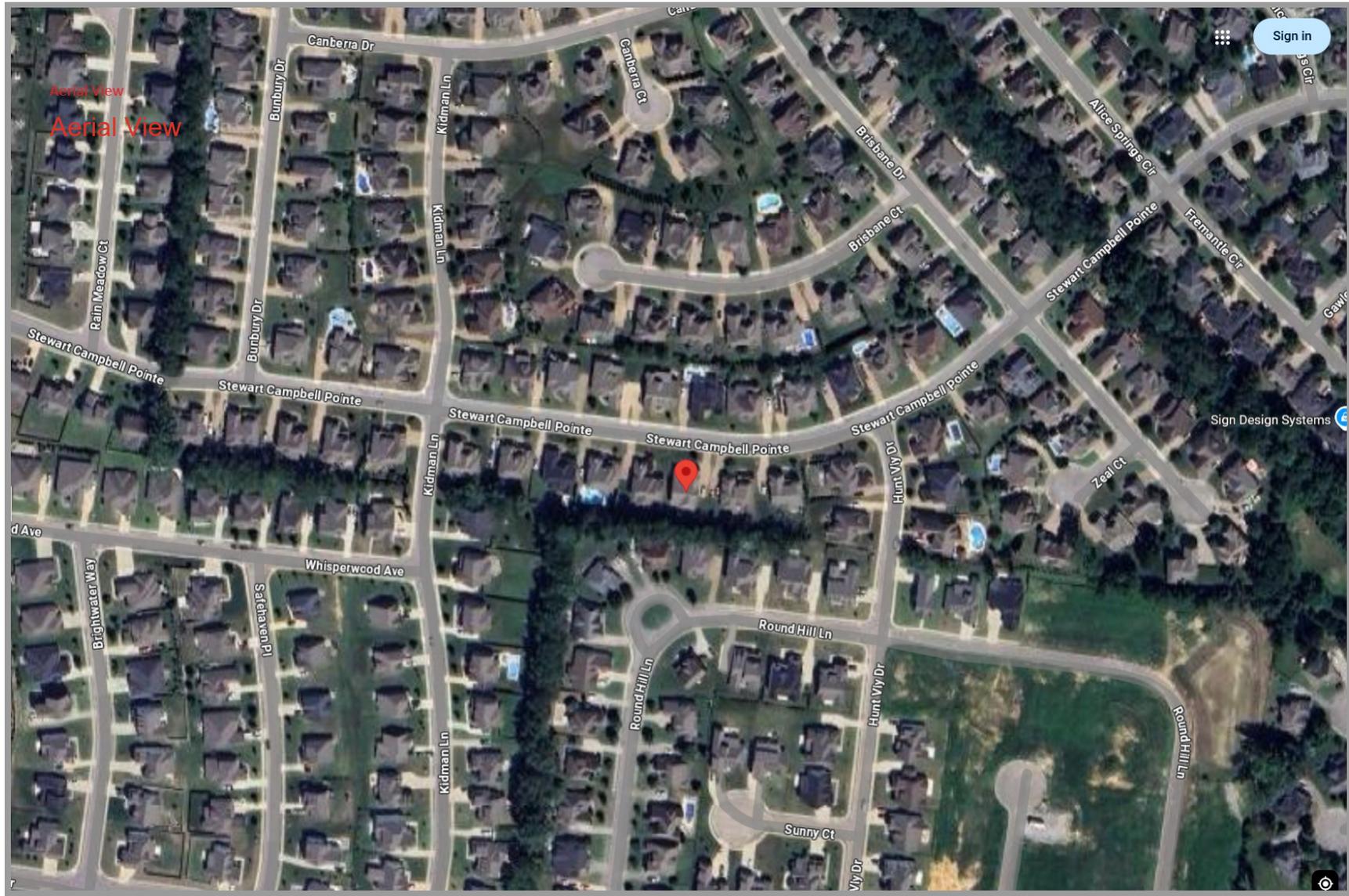
GRAPHIC SCALE 1"=30'



10/03/16

CB
DL
10-6-16







CITY OF SPRING HILL

199 TownCenter Parkway
Spring Hill, TN 37174
931-486-2252 FAX 931-486-3596

POOL-IN GROUND PERMIT
THIS FORM WAS PRINTED ON: 08/06/2025

Permit Number: O-09719-25

Date Issued: 08/06/2025

Project Address: 2981 STEWART CAMPBELL PT 237
CHERRY GROVE
SPRING HILL, TN 37174
Parcel ID:
Subdivision:

Lot #:
Block #:
Zoning:

Owner/Agent: Andrew Kozlowitz
Address: 2981 STEWART CAMPBELL PT 237 CHERRY
GROVE
City: Spring Hill
State: TN
Zip: 37174
Phone: (731) 234-1195

Contractor: Andrew Kozlowitz
Address: 2981 STEWART CAMPBELL PT 237 CHERRY
GROVE
City: Spring Hill
State: TN
Zip: 37174
Phone: (731) 234-1195

Desc of Work: 27x12
Valuation: \$81544.00
Project Sq Ft: 0.00

Fee Code	Quantity	Amount
Pool-In Ground	81,544.00	\$417.72
Total		\$417.72

I HEREBY CERTIFY THAT I HAVE READ AND EXAMINED THIS DOCUMENT AND KNOW THE SAME TO BE TRUE AND CORRECT. ALL PROVISIONS OF LAWS AND ORDINANCES GOVERNING THIS TYPE OF WORK WILL BE COMPLIED WITH WHETHER SPECIFIED HEREIN OR NOT. GRANTING OF A PERMIT DOES NOT PRESUME TO GIVE AUTHORITY TO VIOLATE OR CANCEL THE PROVISION OF ANY OTHER STATE OR LOCAL LAW REGULATING CONSTRUCTION OR THE PERFORMANCE OF CONSTRUCTION.

(Signature of Contractor or Authorized Agent)

Jeanette M.

(Approved By)

____ / ____ / ____
Date
8 / 6 / 25
Date

